

ADDENDUM NO. 2

**Conroe North Houston Regional Airport
Montgomery County, Texas
TxDOT CSJ 1812CONRO
August 7, 2017**

The following addendum to the Contract Documents is hereby incorporated into the project and shall be equally as binding as the original Contract Documents. The contract documents, plans, and specifications for the above project are modified as follows:

1. **Pre-Bid Questions and responses** (questions in non-bold text, responses in bold).
 1. What determines if the Alternate Bid is awarded? **Available funding.**
 2. What is the cutoff date for addenda? **No addenda will be posted later than Tuesday, August 8th.**
 3. What is the fee for getting water? **Water for construction can be obtained from the City of Conroe. A tap is available on Hawthorne Drive. A metered hydrant will be provided by the City for a refundable \$700 deposit and a \$15 service charge. Call 936-522-3170 for complete details.**
 4. What is the largest water line that can be accessed? **Call the City of Conroe, Water Utilities at 936-522-3170.**
 5. Where will water be tapped? **The City of Conroe reserves the right to determine where water for construction is obtained, but most likely it would be on Hawthorne Drive.**
 6. Will excavation spoils be hauled offsite? **No, the airport will designate a location for disposal on site.**
 7. On DBE commitment, when will bidders know how award is going to be regarding 6% or 8%? **At the time of the bid, the Bidder shall adhere to the *Notice to Bidders* using only the Base Bid to meet the 6% goal or Good Faith Effort, and submitted within the 5 calendar day rule. If the Alternate Bid is awarded, an additional 2% DBE goal will be required. The Participation Plan and Commitment form, or Good Faith Effort, shall be submitted to TxDOT Aviation Division with the signed contract. The total goal will then be 8% (Base Bid + Alternate Bid).**
 8. Are there any other governing authorities in the County? **No, but the City of Conroe may check trucks coming to and from the airport for licensing and weight limits.**
 9. Will employees need to be badged? **No. However, all vehicles in the airport operations area (AOA) must be marked, lighted, and properly escorted. A gate guard will be required.**
 10. Will dry lime be used on the project? **No.**
 11. If a batch plant is used, will it need to be located away from airport operations area (AOA)? **Yes, if a concrete batch plant is used for construction, it must be located outside the AOA. A suggested batch plant location on Hawthorne Drive is indicated in the attached revised plan sheet drawing, G8.**
 12. Who is the manufacturer of the existing signs that require panels? If ADB, are they the older "A-Frame" shaped sign? **The following sign numbers are based on the**

field tag column: Sign 1 = ADB (older style); Sign 2 = ADB (older style); Sign 15 = ADB (older style); Sign 37 = Crouse Hinds; Sign 38 = Crouse Hinds; Sign 52 = Lumacurve.

13. Are the sign panels going to be owner furnished since they are sole source and come from three different manufacturers? **No, they are the Contractor's responsibility.**
14. **A copy of the pre-bid sign-in is attached to this addendum. One (1) page is attached.**

2. Instructions to Bidders

Replace Instructions to Bidders Item IB-16 with the following:

IB-16. In accordance with 49 CFR Part 26.53 the bidder/offerer must submit an acceptable DBE plan and commitment or good faith effort no later than 5 calendar days after bid opening as a matter of responsibility. For all federally funded construction projects with a DBE goal, bidders shall use the attached DBE participation plan and appropriate commitment agreement form/s and submit these documents to TxDOT Aviation within 5 calendar days after the bid opening via email to AVNRFQ@txdot.gov. At the time of the bid, the Bidder shall adhere to the Notice to Bidders using only the Base Bid to meet the 6% goal or Good Faith Effort, and submitted within the 5-calendar day rule. If the Alternate Bid is awarded, an additional 2% DBE goal will be required. The Participation Plan and Commitment form, or Good Faith Effort, shall be submitted with the signed contract to TxDOT Aviation for approval. The total goal will then be 8% for the Base Bid and Alternate Bid.

3. Technical Specifications

Include special specification SS-004, Mobilization. The specification is included with this addendum as an attachment. Two (2) pages are attached.

Include special specification SS-005, Field Office Facility. The specification is included with this addendum as an attachment. Two (2) pages are attached.

4. Plans

Replace original plan sheet G4 with the attached revised plan sheet G4. The revised sheet includes the following revisions. Revisions are clouded in the drawing. One (1) page is attached.

Replace General Project Notes #6 with the following: *Water for construction may be obtained from the City of Conroe. A tap is available on Hawthorne Drive, however, the City reserves the right to determine where water for construction is obtained. A metered hydrant will be provided by the City for a refundable \$700 deposit and a \$15 service charge. Call 936-522-3170 for complete details. There shall be no separate pay for providing water for construction, but the cost of this*

work shall be considered subsidiary to the unit prices bid for various items of work in the bid proposal.

Replace General Project Notes #12 with the following: The Contractor shall be responsible for repair of any damage to existing facilities not designated for reconstruction or replacement at his own expense. This includes damages to existing and newly constructed pavements on the airport and existing pavements providing access to and from the airport. Damaged facilities shall be repaired or replaced at the Contractor's expense.

Omit (strike-through) General Project Notes #13, #14, #37, #40, and #42.

Add the following General Project Notes #43: The construction drawings plans as presented represent a complete project. Any item of work shown in the plans but not having a bid item is intended to be incidental to other work items and will not be paid for separately or considered for a change order.

Add the following General Project Notes #44: If the Contractor opts to use an on-site concrete batch plant, he/she shall be required to complete and submit an FAA Form 7460-1, Notice of Proposed Construction or Alteration.

Add the following General Project Notes #45: Equipment intended for use by the Contractor in the AOA shall be limited to a 25 foot maximum height unless prior approval is obtained from the Engineer.

Revise Safety And Security Note #2A as follows: Access to the AOA is restricted. The gate for access to the AOA must remain locked at all times unless an authorized gate guard and flagman is on duty. Contractor vehicles, equipment, personnel and delivery trucks desiring entry to the AOA shall wait for the gate guard to signal them to proceed, and for the flagman to direct them to the work site. The guard and/or flagman shall be equipped with a radio for communication/coordination with the ATCT. Only authorized personnel and equipment will be allowed access to the AOA. The Contractor will furnish training and written procedures to the gate guard and flagman that must be followed during the project.

Revise Safety And Security Note #2G as follows: The Contractor's vehicles operating within the AOA must have a company logo/identification on both sides of the vehicle.

Add Safety and Security Note #10 as follows: The Contractor shall at a minimum have two radios readied for use on the airfield any time that work is in progress.

Revise Operations Notes #6 as follows: A qualified contractor superintendent shall be on site at all times when work is in progress, including the work of subcontractors. The superintendent must be authorized to receive and execute orders of the Engineer. The superintendent shall be responsible for subcontractors.

Revise Operations Notes #9 as follows: Only authorized vehicles will be allowed inside the airport property. Employees must park off-site and enter and leave the airport each day in a company marked vehicle. Company marked vehicle operating inside the AOA must be equipped with an airfield radio and must coordinate with ATCT to cross active pavements.

Replace original plan sheet G7 with the attached revised plan sheet G7. The revised sheet includes a detail for *Taxiway Closure Marking*. Revisions are clouded in the drawing. One (1) page is attached.

Replace original plan sheet G8 with the attached revised plan sheet G8. The revised sheet includes a revised location for the Contractor's construction staging, identification of the Contractor's personnel parking, material storage, and concrete batch plant location, and identification of the Contractor's construction entrance gate. Revisions are clouded in the drawing. One (1) page is attached.

Replace original plan sheet C22 with the following revised plan sheet C22. The revised sheet includes spot elevations for grading out the intersection at TW B. One (1) page is attached.

Replace original plan sheet C24 with the following revised plan sheet C24. The revised sheet includes spot elevations for grading out the intersection at TW F/G. One (1) page is attached.

If you have questions concerning this Addendum No. 2, please contact Franklin Stephens, P.E., of Schaumburg & Polk, Inc., 108 Ida Street, Suite 300, Terrell, TX 75160, 972 563-0205 (office), or 903-262-9937 (cell). All bidders shall acknowledge receipt of this Addendum 2 with your submitted bid.

Ten (10) pages are attached to this addendum.

End of Addendum No. 2



Franklin Stephens, P.E.
August 7, 2017

Montgomery County
 Conroe North Houston Regional Airport
 1812CONRO TW Extension

PRE-BID MEETING

Date: July 26, 2017
 Time: 10:30 a.m.

List of Attendees

NAME	COMPANY REPRESENTED	PHONE NO.	EMAIL ADDRESS
Frank Stephens	SPI Engineers	905 262 9937 972.563.0205	fstephensepi-eng.com
SHERI TOLANAY HCE		281-536-4818	sheri@hollowayconsultingengineers.com
Andrew Cloeter	Spaw Glass	281-970-5300	robin-falke@spawglass.com
BURR GRIFFIN	DUDRAT CONSTRUCTION	979-296-2131	bids@fmdudleyconstruction.com
Alex Rocha	JAMES	832-433-5629	arocha@prim.com
RASY BELANCES	RBR Industrial & Mech	214-205-6468	rasy.belances@RBR-LLC.com
KEVIN KRAMER	G.T.T CONSTRUCTION	281-636-6926	ESTIMATING@GTTCONSTRUCTION.COM
William Key	BLS Construction	979 543 2696	William.Key@blsconstruction.net
Brandon Lewellen	Texas Sterling	281 627 5744	brandon.lewellen@strlco.com
TAMMY JACOB	DREWEY CONST	936-553-9910	TAMMY.JACOB@DREWEYCON.COM
Pam Taras	Montgomery County	936-538-8118	Pam.Taras@metx.org
Eusebio Torres	TKOOT-AN	512-416-4557	Eusebio.Torres@TKOOT.org
DAVID KAHN	HOLES INC	281 475 0869	DAVID@HOLESINC.COM
GARY GERRIG	TRIPLE B SERV. LLP	936-533-3371	gary@triplebservices.com
JAMES BROWN SCOTT SMITH	M.C.	936 780 0311	JAMES.BROWN@MCTX.ORG
Leceeth Ferguson	FC	281-252-9232	Lferguson@fci-engr.com

ITEM SS-004 MOBILIZATION

DESCRIPTION

SS-004-1.1 This item shall consist of work and operations, but is not limited to, necessary for the movement of personnel, equipment, material, and supplies to and from the project site for work on the project, except as provided in the contract as separate pay items.

MEASUREMENT AND PAYMENT

SS-004-2.1 Based upon the contract lump sum price bid for Item SS-004, Mobilization, partial payments will be allowed as follows:

25% of the lump sum amount with the first pay application

An additional 25% of the lump sum amount when 25% or more of the total original contract amount is earned

An additional 40% of the lump sum amount when 50% or more of the total original contract amount is earned

The final 10% of the lump sum amount following final inspection and acceptance of work, submission of final as-built drawings and other close-out documents, and clean-up of staging area and haul route.

END OF ITEM SS-004

INTENTIONALLY LEFT BLANK

ITEM SS-005 FIELD OFFICE FACILITY

DESCRIPTION

SS-005-1.1 This item shall govern for the furnishing of facilities to be used by the Engineer/RPR as a field office during construction of the project.

GENERAL REQUIREMENTS

SS-005-2.1 The facilities shall be furnished subsequent to the Notice to Proceed to begin work, but prior to beginning actual work on the project. The building shall remain in place until the project is accepted as complete, unless otherwise directed by the Engineer. The structure shall be floored, roofed, weather-tight, and constructed in a quality manner.

The building shall be constructed or furnished near the site of the work at a location acceptable to the Engineer. The building may be an independent structure or a part of another structure being utilized by the Contractor. However, direct access between the Engineer's quarters and the Contractor's quarters shall not be allowed. Access to the Engineer's field office shall be by a direct, outside entrance controlled by the Engineer.

If the facility is damaged or destroyed, except when such damage or destruction is due to the negligence of the Engineer, the Contractor shall restore or replace the building as quickly as possible. Upon completion of the project, the Contractor shall remove the facility from the project site and from Airport property.

The building shall be wired for adequate lighting and power supply. The building shall be furnished with a telephone line and internet service. The Contractor shall furnish the facility with a copier that is capable of reproducing 11" x 17" drawings and have scanning and faxing capabilities. The contractor shall provide copy paper as well as ink cartridges. The building shall be heated and cooled. An office chair shall be provided and at least two sitting chairs. A table and workbench shall be provided. A metal filing cabinet with minimum two drawers shall be provided. Steps shall be supplied and installed if needed for access to the building

TYPE OF STRUCTURE

SS-005-3.1 The building shall not be less than 8 feet wide by 16 feet long and 8 feet high, or an approved equivalent. The building shall have not less than four glass windows and one entry/exit door equipped with a lock and key. A workbench and a table measuring approximately 3 feet by 6 feet shall be provided.

The building shall be equipped with indoor plumbing including a toilet and wash basin. Single-phase power, 120 volt, shall be provided. The Contractor is responsible for paying all costs associated with furnishing, installing, leveling, maintaining, and removing the building. The Contractor is responsible for payment of all costs associated with providing and maintaining supply of power, water, and wastewater including all fees and monthly bills.

MEASUREMENT

SS-005-4.1 This item shall not be measured for payment, but shall be considered subsidiary to the unit price bid for Item SS-004, Mobilization by the lump sum.

PAYMENT

SS-005-5.1 This item shall not be measured for payment, but shall be considered subsidiary to the unit price bid for Item SS-004, Mobilization by the lump sum.

END OF ITEM SS-005

GENERAL PROJECT NOTES:

- 1. ANY TEMPORARY FACILITIES SHALL BE COMPLETELY REMOVED FROM THE SITE AT THE COMPLETION OF THE PROJECT AND THE SITE RESTORED TO ITS ORIGINAL CONDITION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGES TO FAA CABLE. CONTRACTOR SHALL CONTACT FAA TECHOPS FOR CABLE LOCATION PRIOR TO DIGGING.
3. CONSTRUCTION PERSONNEL AND EQUIPMENT WILL NOT BE ALLOWED WITHIN THE AIRPORT OPERATIONS AREA (AOA) UNTIL THE AREA HAS BEEN CLOSED TO AIRCRAFT AND NOTAMS ISSUED.
4. NORMALLY, WORK SHALL BE ACCOMPLISHED DURING NORMAL DAYTIME WORKING HOURS ONLY, OR AS APPROVED BY THE ENGINEER, HOWEVER, TO MINIMIZE DISRUPTIONS TO AIRPORT OPERATIONS AND CLOSURE OF RW 1432, WEEKEND AND NIGHT TIME WORK IS ENCOURAGED AND MAY EVEN BE REQUIRED.
5. THE CONTRACTOR SHALL COORDINATE WITH THE ENGINEER AND AIRPORT DIRECTOR ON THE DISPOSAL OF WASTE AND SPOIL MATERIAL ON THE AIRPORT PROPERTY. UNAPPROVED WASTE OR SPOIL MATERIAL SHALL BE DISPOSED OF OFF THE AIRPORT PROPERTY BY THE CONTRACTOR, UNLESS OTHERWISE NOTED. ANY WASTE AND/OR SPOIL MATERIAL PLACED ONTO THE AIRPORT PROPERTY SHALL BE GRADED SMOOTH AND SHALL NOT CAUSE ANY PONDING ON THE AIRPORT PROPERTY NOR CAUSE ANY PROPERTY DAMAGE TO THE AIRPORT OR THE ADJACENT PROPERTY OWNERS. THE CONTRACTOR SHALL NOT PLACE ANY WASTE AND/OR SPOIL MATERIAL IN AREA OR AT HEIGHT WHICH PENETRATES THE PRIMARY SURFACE OR TRANSITIONAL SURFACE FOR EITHER RUNWAY.
6. WATER FOR CONSTRUCTION MAY BE OBTAINED FROM THE CITY OF CONROE. A TAP IS AVAILABLE ON HAWTHORNE DRIVE. HOWEVER, THE CITY RESERVES THE RIGHT TO DETERMINE WHERE WATER FOR CONSTRUCTION IS OBTAINED. A METERED HYDRANT WILL BE PROVIDED BY THE CITY FOR A REFUNDABLE \$700 DEPOSIT AND A \$15 SERVICE CHARGE. CALL 936-522-3170 FOR COMPLETE DETAILS. THERE SHALL BE NO SEPARATE PAY FOR PROVIDING WATER FOR CONSTRUCTION, BUT THE COST OF THIS WORK SHALL BE CONSIDERED SUBSIDIARY TO THE UNIT PRICES BID FOR VARIOUS ITEMS OF WORK IN THE BID PROPOSAL.
7. INSTALL SHORING AND BRACING NECESSARY TO SUPPORT THE SIDES OF THE TRENCHES AND OTHER EXCAVATION WITH VERTICAL SIDES, AS REQUIRED BY THE CURRENT OSHA REGULATIONS. COST OF SHORING AND BRACING SHALL BE SUBSIDIARY TO THE BID PRICE FOR STORM SEWER PIPE.
8. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN ALL HORIZONTAL AND VERTICAL CONSTRUCTION STAKING AS REQUIRED FOR THE PROJECT DEVELOPMENT.
9. THE CONTRACTOR SHALL NOT DEVIATE FROM THE APPROVED CONSTRUCTION SEQUENCE WITHOUT FIRST OBTAINING APPROVAL FROM THE ENGINEER. THE CONTRACTOR SHALL BRING HIS EQUIPMENT AND SHALL KEEP THE EQUIPMENT ON SITE FOR DURATION OF ITS FUNCTION.
10. THE CONTRACTOR IS RESPONSIBLE FOR THE LOCATIONS AND RESTRICTIONS FOR SERVICING AND MAINTAINING EQUIPMENT AND DISPOSAL OF USED LUBRICANTS, ETC. ACCORDING TO THE PERTINENT LAW AND REGULATIONS.
11. NO TRENCHES IN OR DIRECTLY ADJACENT TO OPERATIONAL PAVEMENT SHALL REMAIN OPEN OVERNIGHT OR WHEN THE CONTRACTOR FINISHES WORK FOR THE DAY IN THE AREA. TRENCHES NOT BACKFILLED SHALL BE COVERED WITH STEEL PLATES TO ALLOW SAFE PASSAGE BY AIRCRAFT ACROSS THE TRENCH, IF APPROVED BY THE AIRPORT DIRECTOR.
12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR OF ANY DAMAGE TO EXISTING FACILITIES NOT DESIGNATED FOR RECONSTRUCTION OR REPLACEMENT AT HIS OWN EXPENSE. THIS INCLUDES DAMAGES TO EXISTING AND NEWLY CONSTRUCTED PAVEMENTS ON THE AIRPORT AND EXISTING PAVEMENTS PROVIDING ACCESS TO AND FROM THE AIRPORT. DAMAGED FACILITIES SHALL BE REPAIRED OR REPLACED AT THE CONTRACTOR'S EXPENSE.
13. DAMAGE TO EXISTING PAVEMENTS DUE TO MOVING OR USAGE OF HEAVY EQUIPMENT OR TO THE TRANSPORT OF MATERIALS TO OR ON THE SITE, SHALL BE REPAIRED TO EQUAL OR BETTER CONDITIONS BY THE CONTRACTOR AT HIS OWN EXPENSE.
14. CONSTRUCTION EQUIPMENT AND VEHICLES SHALL TRAVEL A MINIMUM AMOUNT ON NEWLY CONSTRUCTED PAVEMENTS SO THAT THE NEWLY CONSTRUCTED PAVEMENT AREAS WILL NOT BE DAMAGED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR OF ANY DAMAGE TO THE NEWLY CONSTRUCTED PAVEMENTS AT CONTRACTOR'S EXPENSE.
15. ALL ABOVE GROUND ELECTRICAL RELATED APPURTENANCES (IE RUNWAY LIGHTS, SIGNS, CABLE BOXES, CABLE AND/OR DUCT MARKERS, CONDUIT, ETC.) SHALL BE PROTECTED AT ALL TIMES. ANY DAMAGE DONE TO SAID APPURTENANCES BY THE CONTRACTOR SHALL BE REPAIRED TO EQUAL OR BETTER CONDITION AT THE CONTRACTOR'S EXPENSE. THE REPAIRS SHALL BE PERFORMED TO THE SATISFACTION OF THE AIRPORT DIRECTOR, TXDOT AND THE ENGINEER.
16. CONSTRUCTION WORKERS WILL NOT BE ALLOWED TO ESTABLISH OVERNIGHT RESIDENCE ON THE PREMISES. ALL CONSTRUCTION WORKERS SHALL LEAVE THE CONSTRUCTION SITE AT THE END OF THEIR WORK PERIOD.
17. WORK CANNOT COMMENCE IN EACH PHASE UNTIL:
A. SUFFICIENT BARRICADES ARE IN PLACE TO CONFINE THE WORK AREA AND CREATE A BARRIER BETWEEN AIRCRAFT AND VEHICLE MOVEMENT AREAS AND THE CONSTRUCTION AREA.
B. ALL SAFETY EQUIPMENT FOR PERSONNEL AND CONSTRUCTION EQUIPMENT IS IN PLACE AND OPERABLE.
C. ALL RUNWAY AND TAXIWAY SAFETY AREAS HAVE BEEN MARKED AND STAKED. STAKES SHALL BE LOCATED EVERY 500' ON BOTH SIDES OF RUNWAYS AND TAXIWAYS AND MAINTAINED FOR THE ENTIRE COURSE OF THE PROJECT BY THE CONTRACTOR. LOCATING, STAKING, AND ALL ACTIVITIES REQUIRED TO ESTABLISH AND MAINTAIN THE AREAS SHALL BE SUBSIDIARY WORK BY THE CONTRACTOR.
D. THE COMPLETE PROJECT SITE, AND ANY AREAS THAT MAY BE AFFECTED BY THE CONSTRUCTION PROJECT SUCH AS HAUL ROUTES, HAS BEEN VIDEO TAPED BY THE CONTRACTOR AND THE VIDEO PROVIDED TO THE ENGINEER, AIRPORT DIRECTOR AND TXDOT.
E. A NOTICE TO PROCEED HAS BEEN ISSUED BY THE TXDOT AVIATION DIVISION.
22. ALL MATERIAL SUBMITTALS FOR ITEMS TO BE USED IN CONSTRUCTION OF THE PROJECT SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND APPROVAL AT LEAST 10 BUSINESS DAYS PRIOR TO COMMENCEMENT OF WORK.
23. A COMPLETE PROJECT SCHEDULE SHALL BE PROVIDED AT THE PRE-CONSTRUCTION MEETING AND PRESENTED AND EXPLAINED BY THE CONTRACTOR TO THE MEETING ATTENDEES. RUNWAY SHUTDOWN DATES MAY BE DETERMINED AT THE PRE-CONSTRUCTION MEETING, OR AT A LATER TIME, AS APPROVED BY THE ENGINEER AND AIRPORT DIRECTOR.
24. REGULARLY SCHEDULED PROGRESS MEETINGS SHALL BE CONDUCTED AT THE FREQUENCIES AGREED UPON BY THE OWNER, ENGINEER, AND CONTRACTOR. THESE MEETINGS SHALL BE CONDUCTED BY THE ENGINEER.
25. IT IS IMPERATIVE THAT THE PREPARED SUBGRADE BE MAINTAINED IN THE PROPOSED PAVEMENT AREAS AT ALL TIMES. THE CONTRACTOR SHALL SHAPE THE SUBGRADE TO DRAIN AND SHALL PROVIDE, MAINTAIN, AND OPERATE PORTABLE PUMPS AS NECESSARY TO PREVENT THE PONDING OR WATER ON THE SUBGRADE OR BASE COURSE. PUMPING SHALL COMMENCE AS SOON AS POSSIBLE FOLLOWING THE COMPLETION OF A RAINSTORM.
26. STOCKPILES WILL NOT BE PLACED WITHIN ANY WATER OF THE UNITED STATES, INCLUDING WETLANDS.
27. THE PROJECT AREA SHALL BE FREE OF ANY CONTRACTORS STOCKPILE MATERIALS UPON COMPLETION OF THE WORK EXCEPT AS DIRECTED BY THE ENGINEER.
28. PROPER DRAINAGE (NO LOCALIZED PONDING) SHALL BE MAINTAINED, PRIOR TO, DURING AND AFTER MOBILIZATION.
29. THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES ARE SHOWN IN AN APPROXIMATE WAY ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER OR OWNER'S REPRESENTATIVE. ALL UTILITIES AND FACILITIES ARE NOT NECESSARILY SHOWN.
A. THE CONTRACTOR SHALL FIELD LOCATE ALL EXISTING UTILITIES BEFORE COMMENCING WORK. THE CONTRACTOR SHALL CONTACT ALL UTILITY AND FACILITY AGENCIES FOR FIELD MARKING PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL ADVISE THE ENGINEER, IN WRITING, OF ANY EXISTING DAMAGED UTILITIES PRIOR TO BEGINNING CONSTRUCTION.
B. THE CONTRACTOR SHALL PROTECT EXISTING UTILITIES AND FACILITIES, ABOVE OR BELOW GROUND, (IE, AIRPORT LIGHTING, NAV AIDS, ETC.) FROM DAMAGE BY EQUIPMENT OR PERSONNEL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY AND ALL DAMAGES DUE TO THE CONTRACTOR'S FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UTILITIES. ANY UTILITY OR FACILITIES DAMAGED DURING THE PROJECT BY THE CONTRACTOR'S CREWS OR EQUIPMENT SHALL BE PROMPTLY REPAIRED AT THE CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE ENGINEER AND FAA. THE CONTRACTOR SHALL HAND DIG WHEN WITHIN THREE (3) FEET ON ANY KNOWN OR SUSPECTED UNDERGROUND UTILITY.

30. THE OWNER RESERVES THE RIGHT TO CONTRACT AND PERFORM OTHER OR ADDITIONAL WORK ADJACENT TO AND WITHIN THE WORK AREA COVERED BY THIS CONTRACT. WHEN SEPARATE CONTRACTS ARE LET WITHIN THE LIMITS OF ANY ONE PROJECT, EACH CONTRACTOR SHALL CONDUCT HIS WORK SO AS NOT TO INTERFERE WITH OR HINDER THE PROGRESS OF THE COMPLETION OF THE WORK BY THE OTHER CONTRACTORS. THE CONTRACTORS WORKING ON THE SAME PROJECT SHALL COOPERATE WITH EACH OTHER AS ORDERED BY THE ENGINEER. ANY ADDITIONAL EFFORT OR WORK REQUIRED FOR SUCH PAYMENT WILL BE MADE. IT IS NOT ANTICIPATED THAT OTHER WORK MAY BE COMPLETED BY AN INDEPENDENT CONTRACTOR(S) DURING THE COURSE OF THIS PROJECT.

31. FOLLOWING THE PRE-CONSTRUCTION CONFERENCE AND PRIOR TO ANY MOBILIZATION OR COMMENCEMENT OF CONSTRUCTION, THE CONTRACTOR SHALL VIDEO AND PHOTOGRAPH (AS REQUIRED FOR DETAIL) THE ENTIRE CONSTRUCTION WORK AREA. TWO DIGITAL FILES OF THE CONSTRUCTION SITE VIDEO AND PHOTOGRAPHS WILL BE PROVIDED TO THE ENGINEER. SPECIAL ATTENTION TO PAVED ROADS AND AIRFIELD PAVEMENTS WHICH ARE PART OF THE CONTRACTOR'S HAUL ROUTE OR WHICH MUST BE CROSSED TO ACCESS PROPOSED WORK AREAS SHALL BE TAKEN. THE VIDEO AND PHOTOGRAPHS WILL BE USED TO ASSESS DAMAGE, IF ANY, CAUSED BY THE CONTRACTOR'S CREWS AND EQUIPMENT TO EXISTING FACILITIES, UTILITIES AND PAVEMENTS AND DETERMINE THE QUANTITY AND QUALITY OF REQUIRED REPAIR AND RESTORATION.

32. THE CONTRACTOR SHALL SUBMIT, EACH WEEK, A THREE WEEK WRITTEN OR GRAPHICAL ROLLING SCHEDULE TO THE ENGINEER OUTLINING PLANNED AND PROJECTED CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL WEEKLY REVIEW THE PROPOSED SCHEDULE WITH THE ENGINEER, RESIDENT PROJECT REPRESENTATIVE AND AIRPORT MANAGER SO THAT ALL PARTIES ARE INFORMED OF UPCOMING CONSTRUCTION ACTIVITIES. AN UPDATED SCHEDULE MUST BE SUBMITTED WITH EACH PAY REQUEST. PAY REQUESTS SUBMITTED WITHOUT AN UPDATED SCHEDULE WILL BE REJECTED AND RETURNED TO THE CONTRACTOR UNPAID.

33. WEEKLY PROGRESS MEETINGS ARE MANDATORY AND WILL BE SCHEDULED BY THE ENGINEER. THE REGULAR MEETING DATE WILL BE COORDINATED WITH THE AIRPORT MANAGER AND CONTRACTOR BY THE ENGINEER.

34. THE CONTRACTOR SHALL MAINTAIN A CLEAN AND SAFE CONSTRUCTION WORK SITE. THE CONTRACTOR SHALL COMPLETE SITE CLEAN-UP ON A DAILY BASIS AS A MINIMUM. THE CONTRACTOR SHALL PROVIDE A VACUUM TRUCK FOR CLEANING AND REMOVING FOREIGN OBJECT DEBRIS (FOD), DIRT AND OTHER LOOSE MATERIAL FROM THE PROJECT PAVEMENT AREA. THE CONTRACTOR SHALL SUBMIT A FOD PLAN FOR REVIEW AND APPROVAL AT THE PRE-CONSTRUCTION CONFERENCE. WORK ON THE PROJECT MAY NOT COMMENCE WITHOUT AN APPROVE FOD PLAN.

35. NO DEBRIS OF ANY NATURE WILL BE ALLOWED IN ACTIVE AIRCRAFT OPERATIONS AREAS. ALL LOOSE MATERIAL (DIRT, STONE, PAVEMENT, ETC.) MUST BE KEPT WITHIN THE LIMITS OF CONSTRUCTION. IMMEDIATE PRIOR TO MOVING CONSTRUCTION BARRIERS THE AREA OUTSIDE THE BARRICADES SHALL BE CLEARED OF ALL LOOSE MATERIAL. NO LOOSE MATERIALS THAT COULD BLOW INTO AN AIRCRAFT SHALL BE ALLOWED IN THE CONSTRUCTION AREA.

36. THE CONTRACTOR SHALL KEEP ALL AIRFIELD AREAS CLEAN OF DEBRIS, DIRT AND OTHER LOOSE MATERIAL CAPABLE OF CAUSING DAMAGE TO AIRCRAFT. FOD AND DEBRIS SHALL BE IMMEDIATELY AND CONTINUOUSLY REMOVED DURING CONSTRUCTION. THE CONTRACTOR SHALL ALSO MAINTAIN CLEAN AND DEBRIS FREE HAUL ROUTES AND CONSTRUCTION EQUIPMENT.

37. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ACQUIRING ALL WATER REQUIRED FOR CONSTRUCTION ACTIVITIES. NO SEPARATE PAY.

38. ALL SAW-CUTTING REQUIRED FOR COMPLETION OF THE WORK AS PROPOSED SHALL BE CONSIDERED INCIDENTAL TO PROVIDED BID ITEMS. NO SEPARATE PAY.

39. THE CONTRACTOR SHALL ATTEND A PRE-PAVE MEETING PRIOR TO ANY PAVING ON THE PROJECT. CONTRACTOR SHALL ANTICIPATE PRESENTING HIS PLAN FOR PAVING, QUALITY CONTROL, SUPERVISION, ETC. AT THIS MEETING.

40. THE CONTRACTOR SHALL PRESERVE AND/OR PROTECT EXISTING AND NEW PAVEMENTS FROM DAMAGE DUE TO CONSTRUCTION OPERATIONS. EXISTING PAVEMENTS DAMAGED BY THE CONTRACTOR OR CONSTRUCTION ACTIVITIES SHALL BE REPLACED OR REPAVED AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL TAKE IMMEDIATE ACTION TO REMEDY THE DAMAGE. NO SEPARATE PAY.

41. THE CONTRACTOR SHALL PROVIDE A LIGHT WEIGHT STRAIGHT EDGE IN ACCORDANCE WITH THE TECHNICAL SPECIFICATIONS. THE STRAIGHT EDGE SHALL HAVE A HANDLE WITH A TWO (2) FOOT HEIGHT. THE CONTRACTOR SHALL PROVIDE AN EMPLOYEE TO ASSIST THE RESIDENT PROJECT REPRESENTATIVE IN CONDUCTING THE STRAIGHT EDGE TEST REQUIRED BY THE TECHNICAL SPECIFICATIONS.

42. WORK MAY NOT COMMENCE UNTIL:
a. ALL MATERIALS PROPOSED FOR INSTALLATION ON THE PROJECT HAVE BEEN SUBMITTED, REVIEWED, AND APPROVED BY ENGINEER. MATERIAL SUBMITTALS MUST BE MADE 10 DAYS IN ADVANCE OF THEIR PROPOSED USE ON THE PROJECT.
b. A PROJECT SCHEDULE HAS BEEN SUBMITTED AND APPROVED BY THE ENGINEER. THE PROJECT SCHEDULE WILL BE PROVIDED BY THE CONTRACTOR AND DISCUSSED BY ALL ATTENDING PARTIES AT THE PRE-CONSTRUCTION CONFERENCE. THE SCHEDULE MUST INCLUDE IMPORTANT MILESTONE DATES SUCH AS PROPOSED RUNWAY AND TAXIWAY CLOSURES.
c. PROJECT SITE IS VIDEOED AND PHOTOGRAPHED AND DIGITAL COPIES PROVIDED TO THE ENGINEER.
d. SAFETY PLAN IS SUBMITTED AND APPROVED.
e. FOD/DEBRIS PLAN IS SUBMITTED AND APPROVED.
f. COPY OF NOTICE OF INTENT (NOI) AND TCEQ PERMIT IS PROVIDED TO ENGINEER.
g. QUALITY CONTROL PLAN IS SUBMITTED AND APPROVED.
h. EROSION AND SEDIMENT CONTROL MEASURES HAVE BEEN INSTALLED.
i. SUFFICIENT BARRICADES ARE IN PLACE TO SECURE THE WORK AREA AND CREATE A BARRIER BETWEEN AIRCRAFT AND VEHICLE MOVEMENT AREAS AND THE CONSTRUCTION SITE. BARRICADES SHALL BE LIGHTED AT NIGHT.
j. ALL SAFETY MEASURES REQUIRED FOR SAFETY OF PERSONNEL AND EQUIPMENT ARE IN PLACE AND OPERABLE.
k. DIGITAL FILES OF VIDEO AND PHOTOGRAPHS OF THE CONSTRUCTION AREAS HAVE BEEN SUBMITTED TO THE ENGINEER.
l. NOTICE TO PROCEED IS ISSUED BY TXDOT AVIATION DIVISION.

43. THE CONSTRUCTION DRAWINGS PLANS AS PRESENTED REPRESENT A COMPLETE PROJECT. ANY ITEM OF WORK SHOWN IN THE PLANS BUT NOT HAVING A BID ITEM IS INTENDED TO BE INCIDENTAL TO OTHER WORK ITEMS AND WILL NOT BE PAID FOR SEPARATELY OR CONSIDERED FOR A CHANGE ORDER.

44. IF THE CONTRACTOR OPTS TO USE AN ON-SITE CONCRETE BATCH PLANT, HE/SHE SHALL BE REQUIRED TO COMPLETE AND SUBMIT TO THE FAA FORM 7460-1, NOTICE OF PROPOSED CONSTRUCTION OR ALTERATION.

45. EQUIPMENT INTENDED FOR USE BY THE CONTRACTOR IN THE AOA SHALL BE LIMITED TO A 50 FOOT MAXIMUM HEIGHT UNLESS PRIOR APPROVAL IS OBTAINED FOR THE ENGINEER.

SAFETY AND SECURITY NOTES:

1. THE CONTRACTOR'S STAGING AREA FOR OFFICE, STOCKPILE, EQUIPMENT, ENGINEER'S OFFICE, MATERIALS STORAGE AND EMPLOYEE PARKING SHALL BE NO CLOSER THAN 25 FEET FROM ANY FENCE LINE AND OUTSIDE OF THE AIRPORT OPERATIONS AREA (OFA) OF ANY RUNWAY, TAXIWAY OR TAXILANE. STOCKPILE LOCATION TO BE PROVIDED BY AIRPORT STAFF. MAXIMUM STOCKPILE HEIGHT IS 20'. CONTRACTOR SHALL COVER ALL STOCKPILED MATERIAL TO PREVENT WIND EROSION. COVER SHALL BE SECURED IN MANNER THAT PREVENTS COVER FROM BEING BLOWN INTO TRAFFIC AREAS AND BECOMING A SAFETY HAZARD. THE OWNER SHALL APPROVE THE EXACT LOCATION OF THE STAGING AREA AND STOCKPILE AREA PRIOR TO ITS USE.

2. ALL EMPLOYEES OF THE CONTRACTOR SHALL ENTER AND EXIT THE WORK SITE AT THE DESIGNATED CONTRACTOR'S ENTRANCE. AN ADEQUATE SIGN SHALL BE PROVIDED DESIGNATING THIS GATE LOCATION AS THE "CONSTRUCTION EMPLOYEE ENTRANCE." MINIMUM REQUIREMENTS FOR AN ACCESS GATE ARE AS FOLLOWS:

A. ACCESS TO THE AOA IS RESTRICTED. THE GATE FOR ACCESS TO THE AOA MUST REMAIN LOCKED AT ALL TIMES UNLESS AN AUTHORIZED GATE GUARD AND FLAGMAN IS ON DUTY. CONTRACTOR VEHICLES, EQUIPMENT, PERSONNEL AND DELIVERY TRUCKS DESIRING ENTRY TO THE AOA SHALL WAIT FOR THE GATE GUARD TO SIGNAL THEM TO PROCEED, AND FOR THE FLAGMAN TO DIRECT THEM TO THE WORK SITE. THE GUARD AND/OR FLAGMAN SHALL BE EQUIPPED WITH A RADIO FOR COMMUNICATION COORDINATION WITH THE ATCT. ONLY AUTHORIZED PERSONNEL AND EQUIPMENT WILL BE ALLOWED ACCESS TO THE AOA. THE CONTRACTOR WILL FURNISH TRAINING AND WRITTEN PROCEDURES TO THE GATE GUARD AND FLAGMAN THAT MUST BE FOLLOWED DURING THE PROJECT.

B. DELIVERY VEHICLES FOR THE CONTRACTOR NEED TO CHECK IN AT THE GATE. THE CONTRACTOR MUST MAINTAIN POSITIVE ESCORT CONTROL OF ALL DELIVERY VEHICLES WHILE ON SITE.

C. THE GATE GUARD SHALL BE EQUIPPED WITH A TELEPHONE TO CONTACT THE RPR IF ANY UNAUTHORIZED VEHICLE ATTEMPTS TO ENTER THE AIRPORT AIR OPERATIONS AREA.

D. A GATE GUARD IS REQUIRED WHEN THE GATE IS UNLOCKED. THE GATE MAY BE SECURED (CLOSED AND LOCKED) AFTER EACH VEHICLE IN LIEU OF USING A GATE GUARD. THE CONTRACTOR SHALL PROVIDE LOCKS ON ACCESS GATES AND PROVIDE THE COMBINATION TO THE OWNER RPR.

E. ANY AND ALL FINES THAT MAY BE LEVIED ON THE AIRPORT FOR A SECURITY VIOLATION IN CONNECTION WITH THE ACCESS GATE OR THE CONTRACTOR'S ACTIVITIES SHALL BE PAID BY THE CONTRACTOR. THE FAA OR AIRPORT MAY ASSESS A FINE DEPENDING ON THE SERIOUSNESS OF THE INFRACTION.

F. THE CONTRACTOR SHALL AT ALL TIMES ENSURE AGAINST UNAUTHORIZED ACCESS TO THE AIRFIELD.

G. THE CONTRACTOR'S VEHICLES OPERATING WITHIN THE AOA MUST HAVE A COMPANY LOGO IDENTIFICATION ON BOTH SIDES OF THE VEHICLE.

3. ALL CONSTRUCTION VEHICLES OPERATING OR PARKED ON THE AIRPORT MUST HAVE AN OPERATING FLASHING AMBER BEACON OR CHECKERED FLAG (DAYLIGHT ONLY) WHICH IS VISIBLE 360 DEGREES AROUND THE VEHICLE. IN ACCORDANCE WITH THE FAA SPECIFICATION AC 150/5370, LATEST VERSION.

4. IF FOR ANY REASON, THE OWNER OR ENGINEER FEELS THAT SAFETY IS NOT BEING ADEQUATELY MAINTAINED, THEY MAY SUSPEND WORK UNTIL THE SAFETY ISSUE IS RESOLVED.

5. AIRCRAFT AND PUBLIC SAFETY SHALL NOT BE COMPROMISED AND PROPER SEPARATION SHALL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR SHALL BE REQUIRED TO COMPLY WITH ALL REQUIREMENTS OF FAA ADVISORY CIRCULAR 150/5370/2F AND AIRPORT RULES AND REGULATIONS.

6. RADIO COMMUNICATIONS SHALL BE REQUIRED AT ALL TIMES TO MONITOR THE UNICOM (123.0). NO EQUIPMENT OR PERSONNEL SHALL ENTER AN OPEN RUNWAY OR TAXIWAY SAFETY AREA UNLESS PROPERLY COORDINATED WITH THE AIRPORT. THE CONTRACTOR SHALL HAVE AN ADEQUATE NUMBER OF RADIOS ON-SITE AT ALL TIMES AND SHALL BE RESPONSIBLE FOR PROVIDING THE RADIOS. NO SEPARATE PAY.

7. THE CONTRACTOR SHALL BE PREPARED TO IMMEDIATELY CLEAR THE RUNWAY AND/OR TAXIWAY OR EQUIPMENT AND PERSONNEL IN THE EVENT OF A DECLARED AIRCRAFT EMERGENCY.

8. THE CONTRACTOR SHALL BE REQUIRED TO RELOCATE THE CLOSED MARKERS AND AVIATION BARRICADES DURING THE PROJECT. MULTIPLE RELOCATIONS OF THE CLOSED MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT. THE LOCATION OF THE CLOSED MARKERS AND BARRICADES, AS SHOWN OF THE PLANS, REPRESENTS THE MOST TYPICAL LOCATION FOR EACH WORK AREA.

9. THE CONTRACTOR SHALL NOT USE THE AIRPORT ADDRESS FOR MATERIAL DELIVERIES TO THE CONSTRUCTION SITE. THE CONTRACTOR SHALL USE A DELIVERY ADDRESS THE STREET NAME AND NUMBER DETERMINED FOR THE CONTRACTOR'S STAGING AND STORAGE AREA ON SITE. THE CONTRACTOR SHALL PROVIDE PROMINENT SIGN TO IDENTIFY THE CONTRACTOR'S OFFICE AND DELIVERY POINT.

10. THE CONTRACTOR SHALL AT A MINIMUM HAVE TWO RADIOS READIED FOR USE ON THE AIRFIELD ANY TIME THAT WORK IS IN PROGRESS.

OPERATIONS NOTES:

1. THE CONTRACTOR SHALL COMPLETE ALL WORK IN ACCORDANCE WITH THE FAA ADVISORY CIRCULAR 150/5370-2 "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION." LATEST VERSION.

2. THE CONTRACTOR SHALL NOTIFY ENGINEER 48 HOURS IN ADVANCE OF ANY CHANGES IN CONSTRUCTION ACTIVITY TO ALLOW SUFFICIENT TIME FOR COORDINATION WITH AIRPORT MANAGER

3. DURING MATERIAL DELIVERY/PAVING OPERATIONS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING AND MAINTAINING TRAFFIC CONTROL. TRAFFIC CONTROL SHALL BE INCIDENTAL TO THE TOTAL PROJECT.

4. CONSTRUCTION WORK LIMITS ARE AS SHOWN ON THE PLANS. MATERIAL DELIVERIES INTO AOA BY TRUCKING THAT IS NOT CONTRACTOR'S STAFF (HAULERS) SHALL BE ESCORTED BY CONTRACTOR'S STAFF TO THE CONSTRUCTION AREA. ESCORTS ARE TO BE TRAINED AND PROFICIENT IN SPOKEN ENGLISH.

5. ALL CONSTRUCTION VEHICLES AND TRAFFIC (UNLESS OTHERWISE AUTHORIZED) SHALL REMAIN WITHIN THE DESIGNATED CONSTRUCTION LIMITS OR HAUL ROUTES. CONSTRUCTION, STORAGE AND STOCKPILING MUST BE APPROVED BY THE OWNER.

6. A QUALIFIED CONTRACTOR SUPERINTENDENT SHALL BE ON SITE AT ALL TIMES WHEN WORK IS IN PROGRESS, INCLUDING THE WORK OF SUBCONTRACTORS. THE SUPERINTENDENT MUST BE AUTHORIZED TO RECEIVE AND EXECUTE ORDERS OF THE ENGINEER. THE SUPERINTENDENT SHALL BE RESPONSIBLE FOR SUBCONTRACTORS.

7. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATION OF ALL WORK IN ALL DISCIPLINES SHOWN ON THE PLANS. THE CONSTRUCTION PHASING PLAN IS PROVIDED TO ASSIST IN COORDINATING THE CONSTRUCTION.

8. THE CONTRACTOR SHALL SUPPLY SUFFICIENT MANPOWER AND EQUIPMENT REQUIRED TO EFFICIENTLY AND PROMPTLY EXECUTE THE CONSTRUCTION OF THE WORK ITEMS, INCLUDING WORK ON MORE THAN ONE WORK ITEM SIMULTANEOUSLY IN ACCORDANCE WITH THE GENERAL PROVISIONS.

9. ONLY AUTHORIZED VEHICLES WILL BE ALLOWED INSIDE THE AIRPORT PROPERTY. EMPLOYEES MUST PARK OFF-SITE AND ENTER AND LEAVE THE AIRPORT EACH DAY IN A COMPANY MARKED VEHICLE. COMPANY MARKED VEHICLE OPERATING INSIDE THE AOA MUST BE EQUIPPED WITH AN AIRFIELD RADIO AND MUST COORDINATE WITH ATCT TO CROSS ACTIVE PAVEMENTS.

10. NO RUNWAY, TAXIWAY, APRON OR OTHER AIRPORT PAVEMENT/ROADWAY MAY BE CLOSED WITHOUT WRITTEN APPROVAL BY THE ENGINEER. A MINIMUM OF 72 HOURS NOTICE TO ENGINEER IS REQUIRED PRIOR TO ANY PAVEMENT CLOSURE. THE ENGINEER WILL COORDINATE ALL PROPOSED CLOSURES WITH AIRPORT MANAGER.

DEMobilIZATION:

1. UPON COMPLETION OF THE PROJECT, ALL DISTURBED AREAS, INCLUDING THE CONTRACTORS STAGING AREA, HAUL ROUTES, GRADING LIMITS, AND ETC. SHALL BE RESTORED TO ORIGINAL OR BETTER CONDITION, WITH A SMOOTH LINE AND GRADE FOR POSITIVE DRAINAGE. THE CONTRACTOR SHALL SEED AND MULCH ALL DISTURBED AREAS. THERE WILL BE NO MEASUREMENT FOR PAYMENT OF SEEDING AND MULCHING REQUIRED OUTSIDE THE GRADING LIMITS, APPROVED STOCKPILE LIMITS, OR APPROVED HAUL LIMITS. ALL WORK SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.

2. DEMOBILIZATION SHALL BE COMPLETED IN A MANNER THAT MINIMIZES IMPACT TO THE AIRPORT OPERATIONS.

3. SAFETY REGULATIONS SHALL BE OBSERVED AND MAINTAINED DURING DEMOBILIZATION.

ENVIRONMENTAL:

1. IN THE EVENT THAT UNANTICIPATED ARCHEOLOGICAL DEPOSITS ARE ENCOUNTERED DURING CONSTRUCTION, WORK IN THE IMMEDIATE AREA WILL CEASE AND THE TXDOT PROJECT MANAGER WILL CONTACT PROFESSIONAL ARCHEOLOGISTS TO INITIATE POST-REVIEW DISCOVERY PROCEDURES UNDER THE PROVISIONS OF 38 CR 800.13.

2. IN THE EVENT THAT UNANTICIPATED HAZARDOUS MATERIAL ARE ENCOUNTERED DURING CONSTRUCTION, THE WORK IN THE IMMEDIATE AREA WILL CEASE AND THE TXDOT PROJECT MANAGER WILL BE NOTIFIED.

3. HAZARDOUS WASTE: ALL SPILLS MUST BE REPORTED IMMEDIATELY TO THE TXDOT PROJECT MANAGER.

4. AN INSPECTION WILL BE PERFORMED BY A RESIDENT PROJECT REPRESENTATIVE EVERY SEVEN DAYS. AN INSPECTION REPORT WILL BE MADE FOR EACH INSPECTION. BASED ON INSPECTION RESULTS, THE CONTROLS SHALL BE REVISED PER THE INSPECTION REPORT.

5. STOCKPILES, STAGING AREAS AND DISPOSAL AREAS WILL NOT BE PLACED WITHIN ANY WATER OF THE UNITED STATES, INCLUDING WETLANDS AND STREAM BEDS, DISPOSAL AREAS, STOCKPILES, AND HAUL ROUTES SHALL BE CONSTRUCTED IN A MANNER THAT WILL MINIMIZE AND CONTROL THE SEDIMENT. CONSTRUCTION STAGING AREAS AND VEHICLE MAINTENANCE AREAS SHALL BE CONSTRUCTED BY THE CONTRACTOR IN A MANNER TO MINIMIZE THE RUNOFF OF POLLUTANTS. ALL WATERWAYS SHALL BE CLEARED AS SOON AS PRACTICAL OF TEMPORARY EMBANKMENT, TEMPORARY BRIDGES, MATTING, FALSE WORK, PILING, DEBRIS, OR OTHER OBSTRUCTIONS PLACE DURING CONSTRUCTION OPERATION THAT ARE NOT A PART OF THE FINISHED WORK.

6. TPDES:
a. RECEIVING WATER: SHOW THE RECEIVING WATERS ON ALL APPLICABLE PLAN SHEETS AND THE EROSION CONTROL SHEETS. IT IS BEST TO CREATE A LAYER FOR THE RECEIVING WATERS, WITH ARROWS THAT INDICATE THE FLOW PATTERNS.
b. INSPECTION: THIS MUST OCCUR EITHER:
i. EVERY SEVEN DAYS, OR
ii. EVERY 14 DAYS AND AFTER EACH 0.5-INCH RAIN

7. MIGRATORY BIRD TREATY ACT: IF THERE IS A BURROWING OWL ON THE NEST, AND THAT FEMALE DOES NOT LEAVE THE NEST WHEN APPROACHED, ASSUME THERE ARE EGGS OR CHICK IN THE HOLD REGARDLESS OF THE TIME OF YEAR. NO WORK MAY OCCUR WITHIN 100 FEET OF AN ACTIVE NEST. THE ACTIVE NEST(S) MUST BE PROTECTED BY ORANGE MESH SAFETY FENCING.

8. THE FEDERAL MIGRATORY BIRD TREATY ACT (MBTA) (16 USC 703-711), 50 CFR 10, AND FISH AND GAME CODE 3503, 3513, AND 3800, PROTECT MIGRATORY AND NONGAME BIRDS, THEIR OCCUPIED NEST, AND THEIR EGGS. IN THE EVENT THAT MIGRATORY BIRDS ARE ENCOUNTERED ON-SITE DURING PROJECT CONSTRUCTION FROM FEBRUARY 15 TO OCTOBER 1, THE SUPERINTENDENT WORKING ON THE PROJECT MUST TAKE REASONABLE CARE TO AVOID IMPACTS TO THE PROTECTED BIRDS, ACTIVE NESTS, EGGS, AND/OR THE YOUNG. A PERSON THAT VIOLATES THE MBTA MAY BE HELD STRICTLY LIABLE FOR THE ACTIONS THAT RESULT IN AN UNPERMITTED TAKE. WHEN MIGRATORY BIRD NEST ARE DISCOVERED WHICH MAY BE ADVERSELY AFFECTED THE CONSTRUCTION ACTIVITY, OR WHEN A BIRD IS FOUND INJURED OR KILLED AS A RESULT OF CONSTRUCTION ACTIVITY, IMMEDIATELY STOP WORK WITHIN 50 FEET OF THE NEST OR BIRD AND NOTIFY THE RPR/ENGINEER AND TXDOT ENVIRONMENTAL PLANNER.



SCHAUMBURG & POLK, INC.
BEAUMONT • HOUSTON • TYLER
ADDRESS:
320 S. Broadway,
Suite 200
Tyler, TX 75702
TELEPHONE:
903-595-3913
FAX:
903-595-2093

Firm Regist. #: F-000520
Copyright 2017



Franklin Stephens PE
8/7/2017

PROJECT NAME:
CONROE
NORTH
HOUSTON
REGIONAL
AIRPORT
TAXIWAY
EXTENSION

1812CONRO

SHEET NAME:

PROJECT
NOTES

DATE:
07/13/2017

REVISION 1:
ADDENDUM #2
AUGUST 7th, 2017

REVISION 2:
--

REVISION 3:
--

REVISION 4:
--

REVISION 5:
--

DRAWN BY:
T. REED

REVIEWED BY:
F. STEPHENS

PROJECT NO.
930101.00

SHEET NO.

G4

PLT STYLE: SPOLOD.CTB

PLT DATE: 8/7/2017 9:33 AM

FILENAME: X:\CLIENTS\MONTGOMERY COUNTY\930101.00-AIRPORT-TXDOT\DRAWINGS\00-PLANS\001-004-GENERAL-SHEETS.DWG

CONSTRUCTION SEQUENCING PLAN - TW EXTENSION PROJECT
CONROE NORTH HOUSTON REGIONAL AIRPORT (1812CONRO)

GENERAL NOTES FOR CONSTRUCTION SEQUENCING:

1. WORK TO COMPLETE CONSTRUCTION ON THE PROJECT WILL REQUIRE MULTIPLE CLOSINGS OF TWS AND RWS. INSTALLING, REMOVING AND REINSTALLING BARRICADES AND CLOSED RW MARKERS MULTIPLE TIMES WILL BE REQUIRED. NO SEPARATE PAY SHALL BE MADE FOR THIS WORK BUT THE COST SHALL BE SUBSIDIARY TO THE UNIT PRICE BID FOR BARRICADES AND TRAFFIC CONTROL.
2. THE CONTRACTOR IS HEREBY NOTIFIED THAT WORKING ON A PULL-BACK BASIS WITHIN THE AIRPORT'S RSAS AND TSAS IS NO LONGER ALLOWED PER FAA RULES.
3. THE CONTRACTOR SHALL CLEARLY MARK THE RSAS AND TSAS ASSOCIATED WITH EACH PHASE OF WORK PRIOR TO BEGINNING WORK IN EACH PHASE. THE PURPOSE OF THIS DELINEATION IS TO CLEARLY IDENTIFY THE AREAS IN WHICH WORKERS AND EQUIPMENT ARE TO STAY CLEAR OF. THE RSA AND TSA MARKINGS SHALL BE CLEARLY MAINTAINED THROUGHOUT THE COURSE OF WORK ON EACH PHASE FOR WHICH THE MARKS ARE REQUIRED. MARKING AND MAINTAINING THE MARKINGS FOR RSAS AND TSAS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED SUBSIDIARY TO THE UNIT PRICE BID FOR BARRICADES AND TRAFFIC CONTROL.
4. TRAFFIC SHALL NOT BE ALLOWED ON COMPLETED CONCRETE PAVEMENT UNTIL A COMPRESSIVE STRENGTH OF 3,300 PSI HAS BEEN ATTAINED.
5. THE CONTRACTOR MAY SUBMIT AN ALTERNATE PLAN OF SEQUENCING FOR CONSIDERATION AND APPROVAL. THE ALTERNATIVE PLAN MUST BEAR THE SIGNATURE AND SEAL OF A REGISTERED PROFESSIONAL ENGINEER LICENSED TO PRACTICE IN THE STATE OF TEXAS.
6. SHORT-TERM CLOSURES (LESS THAN ONE DAY) OF THE RUNWAYS AND/OR TAXIWAYS SHALL BE COORDINATED WITH THE AIR TRAFFIC CONTROL TOWER (ATCT) AND AIRPORT MANAGER. SHORT-TERM CLOSURES SHALL BE SCHEDULED AT LEAST 48 HOURS IN ADVANCE.
7. NO TAXIWAY CAN BE IN OPENED TO AIRCRAFT TRAFFIC WHEN EDGE DROP-OFFS GREATER THAN 1 1/2 INCHES EXIST. EDGE TREATMENT AND SLOPE TREATMENT DETAILS ARE INCLUDED ON THIS SHEET.
8. AFTER TEMPORARY ACCESS/HAUL ROAD AND STAGING AREA ARE REMOVED AT COMPLETION OF THE PROJECT, THE AREAS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AND RE-SEED. THERE IS NO SEPARATE BID TO REMOVE, DISPOSE OF, AND RE-SEED THE ACCESS/HAUL ROAD AND STAGING AREA. THIS WORK IS SUBSIDIARY TO THE UNIT PRICE BID FOR "TEMPORARY ACCESS/HAUL ROAD AND STAGING AREA."

PHASE 1 - CONSTRUCT TEMPORARY ACCESS/HAUL ROAD AND CONSTRUCTION STAGING AREA. DURATION IS LIMITED TO 20 CALENDAR DAYS.

STEP 1: CONSTRUCT TEMPORARY ACCESS/HAUL ROAD AND CONTRACTOR'S STAGING AREA FOR CONSTRUCTION. COORDINATE WITH ATCT AND AIRPORT TO OBTAIN TEMPORARY CLOSURES OF TWG, TW F, TW B, AND RW 1/19 AS NEEDED TO CONSTRUCT STAGING AREA AND HAUL ROAD. ENSURE THAT SLOPES WITHIN RSA AND TSAS MEET FAA REQUIREMENTS.

STEP 2: INSTALL TEMPORARY CULVERTS TO PROVIDE DRAINAGE ACROSS HAUL ROAD AS NECESSARY DURING CONSTRUCTION.

PHASE 2 - CONSTRUCT STORM SEWER, GRADING, SUBGRADE, ELECTRICAL TRENCHING/ SUBSURFACE WORK, AND CONCRETE TREATED BASE (CTB) ON TW EXTENSION. CONSTRUCT GRADING, SUBGRADE, ELECTRICAL TRENCHING/ SUBSURFACE WORK, AND CONCRETE TREATED BASE (CTB) FOR NEW EXIT TW. CONSTRUCT CONCRETE PAVING FOR NEW EXIT TW BETWEEN RW 14/32 AND EXIT TW STA. 1+30. DURATION IS LIMITED TO 120 CALENDAR DAYS.

STEP 1: INSTALL INLET GI-A05, STORM DRAIN LINE A-05, AND MAKE CONNECTION OF LINE A-05 TO EXISTING INLET GI-A04. TEMPORARY CLOSURE OF RW 14/32 WILL BE REQUIRED TO CONNECT LINE A-05 WITH THE EXISTING INLET. COORDINATE TEMP. CLOSURE OF RW 14/32 WITH THE ATCT AND THE AIRPORT.

IF ALTERNATE BID AWARDED: DEMO EXISTING INLET AND APPROXIMATELY 64 LF OF EXISTING 24" RCP AT TW EXT STA 36+92. INSTALL INLET GI-C01 AND CONNECT EXISTING 24" RCP TO NEW INLET.

STEP 2: CLOSE TWA. CONSTRUCT GRADING, SUBGRADE, CTB, AND ELECTRICAL TRENCHING/ SUBSURFACE WORK BETWEEN BEGINNING OF PROJECT STA 4+60.40 AND APPROX. STA 5+80 (STAY CLEAR OF RSA). INSTALL TEMPORARY SLOPE TREATMENT FROM EXISTING TWA PAVEMENT EDGE TO APPROXIMATE STA 5+10 (BEYOND TW A TSA). REOPEN TWA.

STEP 3: CLOSE RW 1/19 AND TW B. CONSTRUCT GRADING, SUBGRADE, CTB, AND ELECTRICAL TRENCHING/SUBSURFACE WORK BETWEEN APPROXIMATE STA 5+80 AND STA 31+00 (END OF BASE BID). IF ALT BID AWARDED, EXTEND WORK TO APPROXIMATE STA 38+00 (STAY CLEAR OF TW F/G TSA).

INSTALL LINE B-03 AND INLET GI-B03. CONNECT LINE B-03 TO EXISTING INLET GI-B01. DEMO AND RECONSTRUCT TWB TIE-INS THROUGH CTB. INSTALL TEMPORARY SLOPE TREATMENTS AT EXISTING RW 1/19 PAVEMENT EDGES.

CONSTRUCT NEW EXIT TW GRADING, SUBGRADE, CTB, AND ELECTRICAL TRENCHING/ SUBSURFACE WORK BETWEEN TW EXTENSION AND APPROXIMATE EXIT TW STA 1+30 (STAY CLEAR OF RW RSA). RE-OPEN RW 1/19. TWB TO REMAIN CLOSED BETWEEN RW 1/19 AND RW 14/32.

STEP 4: CLOSE RW 14/32. CONSTRUCT GRADING, SUBGRADE, CTB, ELECTRICAL TRENCHING/SUBSURFACE WORK, AND NEW CONCRETE PAVEMENT FOR NEW EXIT TW BETWEEN APPROXIMATE EXIT TW STA 1+30 AND RW 14/32 PAVEMENT EDGE. STEP 4 SHALL BE SEQUENCED SUCH THAT WORK IS PERFORMED ONLY ON WEEKENDS AND NIGHTS. REOPEN RW 14/32 AT THE END OF EACH NIGHT/ WEEKEND SEQUENCE AND AT THE COMPLETION OF ALL STEP 4 WORK. TEMPORARY SLOPE TREATMENTS SHALL BE INSTALLED PRIOR TO REOPENING RW 14/32 AT THE END OF EACH WORK SUBSEQUENCE. CLOSE NEW EXIT TW SO THAT AIRCRAFT DO NOT ATTEMPT TO USE.

PHASE 3 - CONSTRUCT CONCRETE PAVING ON TW EXTENSION. DURATION IS LIMITED TO 100 CALENDAR DAYS.

STEP 1: CLOSE TW A. CONSTRUCT CONCRETE PAVING BETWEEN BEGINNING OF PROJECT STA 4+60.40 AND APPROX. STA 5+80 (CLEAR TW A TSA AND STAY CLEAR OF RSA). REMOVE AND HAUL OFF TEMPORARY HAUL ROAD BETWEEN BEGINNING OF PROJECT STA 4+60.40 AND APPROX. STA 5+80 (CLEAR TW A TSA AND STAY CLEAR OF RSA). PERMANENT SEED/SOD ALL DISTURBED AREAS ASSOCIATED WITH PAVED AREA. RE-OPEN TW A.

STEP 2: CLOSE RW 1/19. CONSTRUCT CONCRETE PAVING BETWEEN APPROX. STA 5+80 AND APPROX. STA 10+00 (CLEAR RW 1/19 RSA). REMOVE AND HAUL OFF TEMPORARY HAUL ROAD BETWEEN APPROX. STA 5+80 AND APPROX. STA 10+00 (CLEAR RW 1/19 RSA). PERMANENT SEED/SOD ALL DISTURBED AREAS ASSOCIATED WITH PAVED AREA. RE-OPEN RW 1/19.

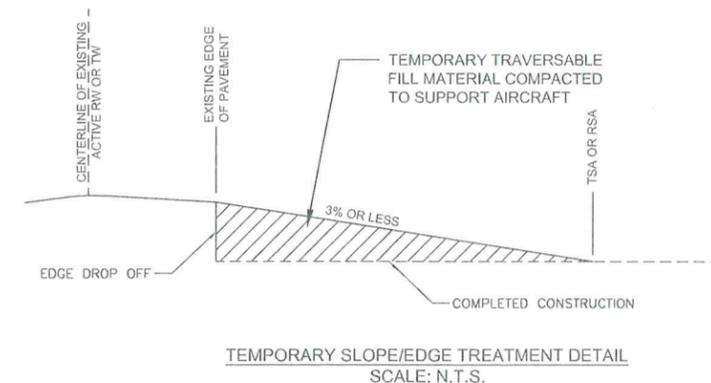
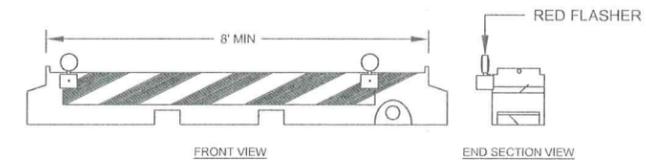
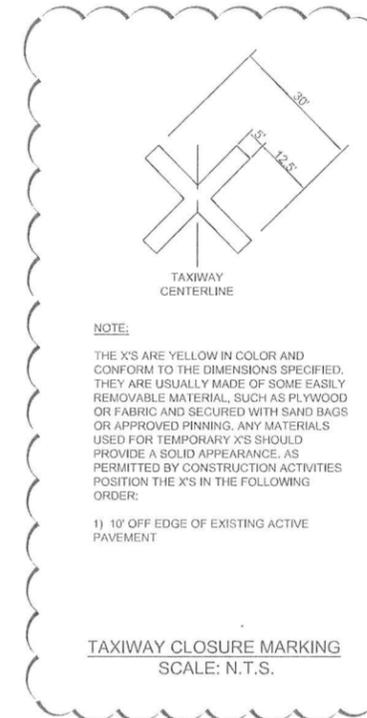
STEP 3: TWB BETWEEN THE TWO RUNWAYS HAS BEEN CLOSED SINCE STEP 3 OF PHASE 2. CONTINUE TW B CLOSURE. CONSTRUCT CONCRETE PAVING BETWEEN APPROX. STA 10+00 AND STA 31+00 (END OF BASE BID). CONSTRUCT CONCRETE PAVING FOR TW B INTERSECTION AND COMPLETE TIE-IN. CONSTRUCT CONCRETE PAVING FOR EXIT TW TIE-IN. REMOVE AND HAUL OFF TEMPORARY HAUL ROAD BETWEEN APPROX. STA 10+00 AND APPROX. STA 31+00 (END OF BASE BID). PERMANENT SEED/SOD ALL DISTURBED AREAS ASSOCIATED WITH PAVED AREAS. INSTALL PAVEMENT MARKINGS FOR NEW TW EXTENSION, NEW EXIT TW, AND RECONSTRUCTED TW B. IF ALT BID AWARDED, EXTEND WORK TO APPROXIMATE STA 38+00 (STAY CLEAR OF TW F/G TSA). RE-OPEN TW B, OPEN NEW EXIT TW, AND OPEN NEW TW EXTENSION BETWEEN THE NEW EXIT TW AND EXISTING TW A.

PHASE 4 - PHASE 4 WORK TO BE PERFORMED ONLY IF ALTERNATE BID IS AWARDED. DURATION IS LIMITED TO 50 CALENDAR DAYS.

STEP 1: CLOSE TW F BETWEEN BLACK FOREST PARKING APRON NORTH ENTRANCE/EXIT AND RW 14/32. CLOSE TW G BETWEEN BLACK FOREST PARKING APRON EAST ENTRANCE/EXIT AND TW F. AIRCRAFT DESIRING TO EXIT RW 14/32 AT TW F OR TW G WILL BE REQUIRED TO BACK-TAXI TO THE NEW EXIT TW AND ON TO THE NEW TW EXTENSION NORTHWARD. CONSTRUCT GRADING, SUBGRADE, CTB, ELECTRICAL TRENCHING/SUBSURFACE WORK, AND NEW CONCRETE PAVEMENT BETWEEN APPROXIMATE STA 38+00 AND TW F/G. CONSTRUCT CONCRETE PAVING FOR TW F/G INTERSECTION AND COMPLETE TIE-INS.

STEP 2: REMOVE AND HAUL OFF TEMPORARY HAUL ROAD AND STAGING AREA BETWEEN APPROX. STA 38+00 AND TW F/G INTERSECTION. PERMANENT SEED/SOD ALL DISTURBED AREAS ASSOCIATED WITH PAVED AREAS.

STEP 3: INSTALL PAVEMENT MARKINGS FOR NEW TW EXTENSION FROM APPROX. STA 38+00 TO TW F/G INTERSECTION AND ON RECONSTRUCTED PORTIONS OF TW F AND TW G. RE-OPEN TW F AND TW G.



ADDRESS:
320 S. Broadway,
Suite 200
Tyler, TX 75702
TELEPHONE:
903-595-3913
FAX:
903-595-2093

Firm Regist. #: F-000520
© Copyright 2017



PROJECT NAME:
CONROE
NORTH
HOUSTON
REGIONAL
AIRPORT

TAXIWAY
EXTENSION
1812CONRO

SHEET NAME:

CONSTRUCTION
SEQUENCE &
SAFETY
PLAN
PHASE
NARRATIVE
&
DETAILS

DATE:
07/19/2017

REVISION 1:
ADDENDUM #2
AUGUST 7th, 2017

REVISION 2:
--

REVISION 3:
--

REVISION 4:
--

REVISION 5:
--

REVISION 6:
--

DRAWN BY:
T. REED

REVIEWED BY:
F. STEPHENS

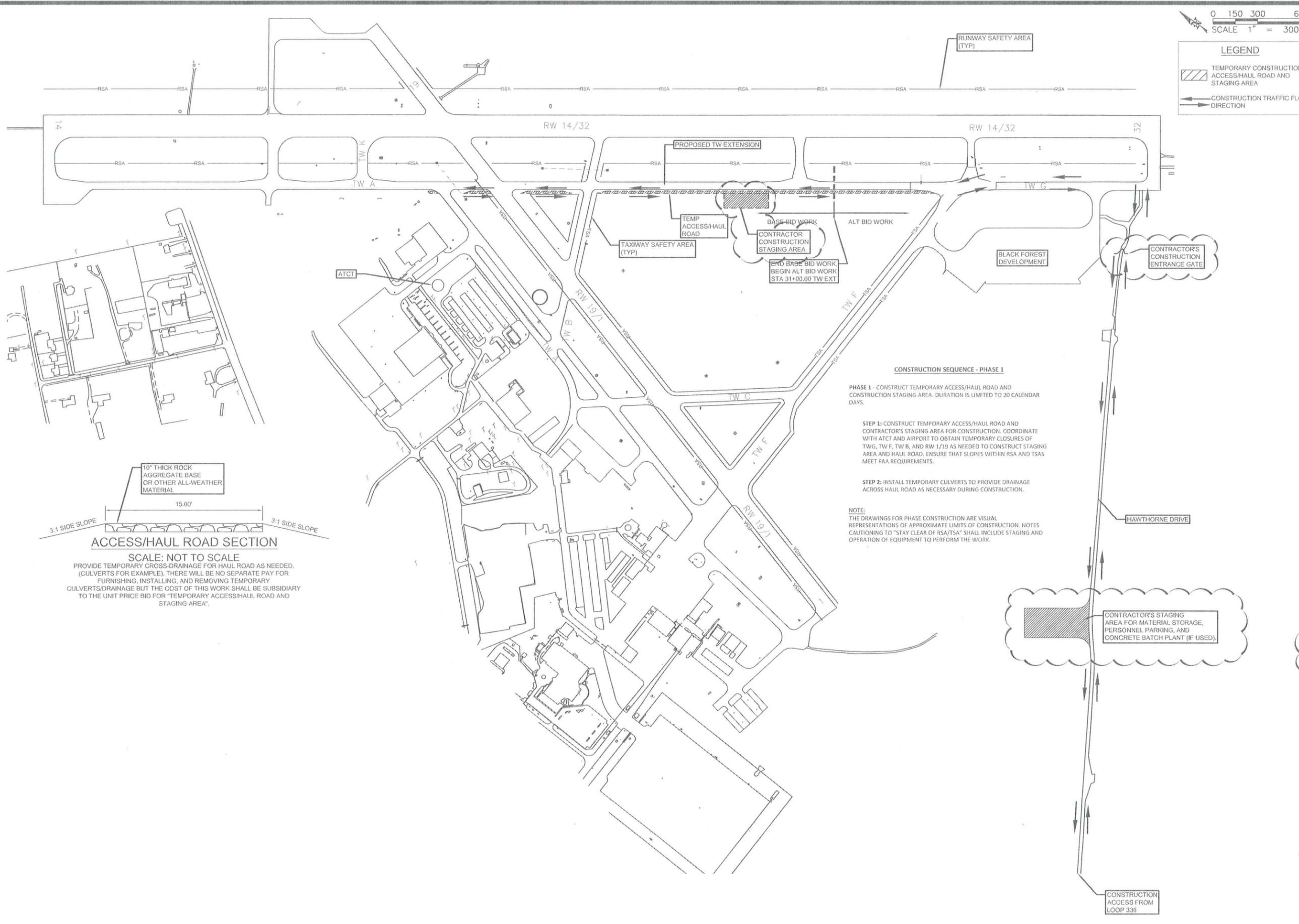
PROJECT NO.
930101.00

SHEET NO.

G7

FILENAME: X:\CLIENTS\MONTGOMERY COUNTY\30101.00-AIRPORT TIE-IN DRAWINGS\00-PLANS\007-G11 CONSTRUCTION SEQUENCE & SAFETY PLAN.DWG PLOT DATE: 8/7/2017 9:24 AM PLOT STYLE: SP10.DCTB

FILENAME: X:\CLIENTS\MONTGOMERY COUNTY\930101.00-AIRPORT TWEAK DRAWINGS\00-PLANS\007-011 CONSTRUCTION SEQUENCE & SAFETY PLAN.DWG PLOT DATE: 8/7/2017 9:34 AM PLOT STYLE: SPD.DCTB



0 150 300 600
SCALE 1" = 300'

LEGEND

- TEMPORARY CONSTRUCTION ACCESS/HAUL ROAD AND STAGING AREA
- CONSTRUCTION TRAFFIC FLOW DIRECTION

SPIX
SCHAUMBURG & POLK, INC.
BEAUMONT • HOUSTON • TYLER

ADDRESS:
320 S. Broadway,
Suite 200
Tyler, TX 75702

TELEPHONE:
903-595-3913

FAX:
903-595-2093

Firm Regist. #: F-000520
© Copyright 2017



PROJECT NAME:
CONROE NORTH HOUSTON REGIONAL AIRPORT

TAXIWAY EXTENSION

1812CONRO

SHEET NAME:

CONSTRUCTION SEQUENCE & SAFETY PLAN

PHASE 1

DATE:
07/13/2017

REVISION 1:
ADDENDUM #2
AUGUST 7th, 2017

REVISION 2:
--

REVISION 3:
--

REVISION 4:
--

REVISION 5:
--

DRAWN BY:
T. REED

REVIEWED BY:
F. STEPHENS

PROJECT NO.
930101.00

SHEET NO.
G8



ACCESS/HAUL ROAD SECTION
SCALE: NOT TO SCALE
PROVIDE TEMPORARY CROSS-DRAINAGE FOR HAUL ROAD AS NEEDED. (CULVERTS FOR EXAMPLE). THERE WILL BE NO SEPARATE PAY FOR FURNISHING, INSTALLING, AND REMOVING TEMPORARY CULVERTS/DRAINAGE BUT THE COST OF THIS WORK SHALL BE SUBSIDIARY TO THE UNIT PRICE BID FOR "TEMPORARY ACCESS/HAUL ROAD AND STAGING AREA".

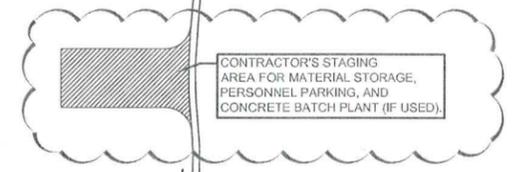
CONSTRUCTION SEQUENCE - PHASE 1

PHASE 1 - CONSTRUCT TEMPORARY ACCESS/HAUL ROAD AND CONSTRUCTION STAGING AREA. DURATION IS LIMITED TO 20 CALENDAR DAYS.

STEP 1: CONSTRUCT TEMPORARY ACCESS/HAUL ROAD AND CONTRACTOR'S STAGING AREA FOR CONSTRUCTION. COORDINATE WITH ATCT AND AIRPORT TO OBTAIN TEMPORARY CLOSURES OF TW G, TW F, TW B, AND RW 1/19 AS NEEDED TO CONSTRUCT STAGING AREA AND HAUL ROAD. ENSURE THAT SLOPES WITHIN RSA AND TSA MEET FAA REQUIREMENTS.

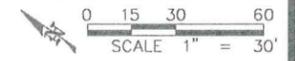
STEP 2: INSTALL TEMPORARY CULVERTS TO PROVIDE DRAINAGE ACROSS HAUL ROAD AS NECESSARY DURING CONSTRUCTION.

NOTE:
THE DRAWINGS FOR PHASE CONSTRUCTION ARE VISUAL REPRESENTATIONS OF APPROXIMATE LIMITS OF CONSTRUCTION. NOTES CAUTIONING TO "STAY CLEAR OF RSA/TSA" SHALL INCLUDE STAGING AND OPERATION OF EQUIPMENT TO PERFORM THE WORK.



CONSTRUCTION ACCESS FROM LOOP 336

FILENAME: X:\JENNIS\MOKCOMERY\COUNTY\30101.00-ARRPORT\TEXT\DRAWINGS\00-PLANS\C21-C24-DIMENSION CONTROL PLANDWG PLOT DATE: 8/4/2017 10:08 AM PLOT STYLE: SPLOD.CTB



NOTES:

1. TAXIWAY DESIGN IS TDG 3 PER AC 150/5300-13A.
2. BACKGROUND GRID FOR PROPOSED PAVEMENT IS FROM PAVING JOINT PLAN FOR REFERENCE. TYPICAL GRID SQUARES ARE 12.5' X 12.5'.
3. TOP NUMBER IN COORDINATE LABELS ARE NORTHING, BOTTOM NUMBERS ARE EASTING.

SPI
 SCHAUMBURG & POLK, INC.
 BEAUMONT • HOUSTON • TYLER

ADDRESS:
 320 S. Broadway,
 Suite 200
 Tyler, TX 75702

TELEPHONE:
 903-595-3913

FAX:
 903-595-2093

Firm Reg. #: F000520
 © Copyright 2017



PROJECT NAME:
CONROE NORTH HOUSTON REGIONAL AIRPORT

TAXIWAY EXTENSION

1812CONRO

SHEET NAME:

DIMENSION CONTROL PLAN

STA 8+50 TO STA 16+50

DATE:
 07/19/2017

REVISION 1:
 ADDENDUM #2
 AUGUST 7th, 2017

REVISION 2:
 --

REVISION 3:
 --

REVISION 4:
 --

REVISION 5:
 --

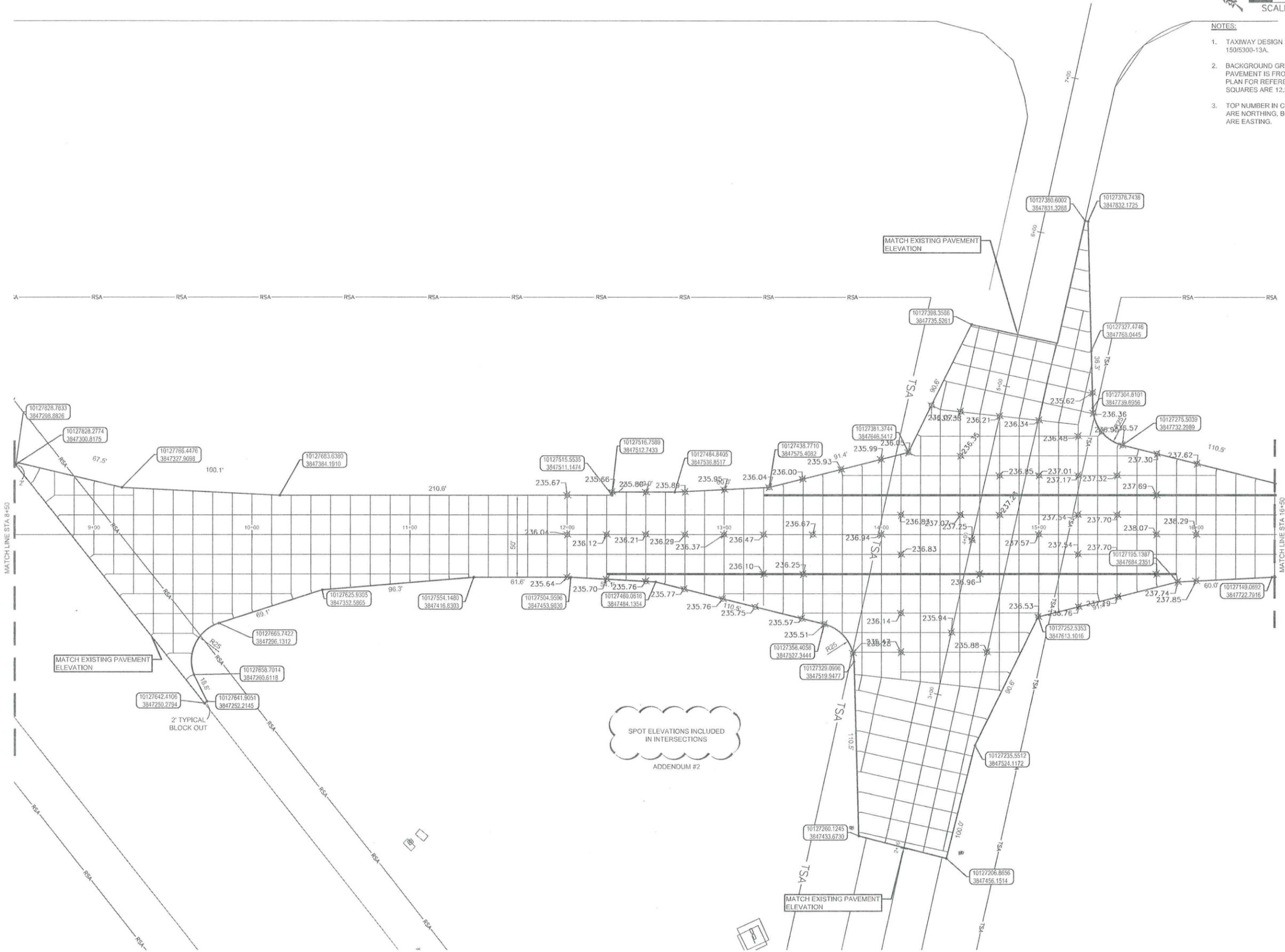
DRAWN BY:
 T. REED

REVIEWED BY:
 F. STEPHENS

PROJECT NO.
 930101.00

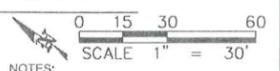
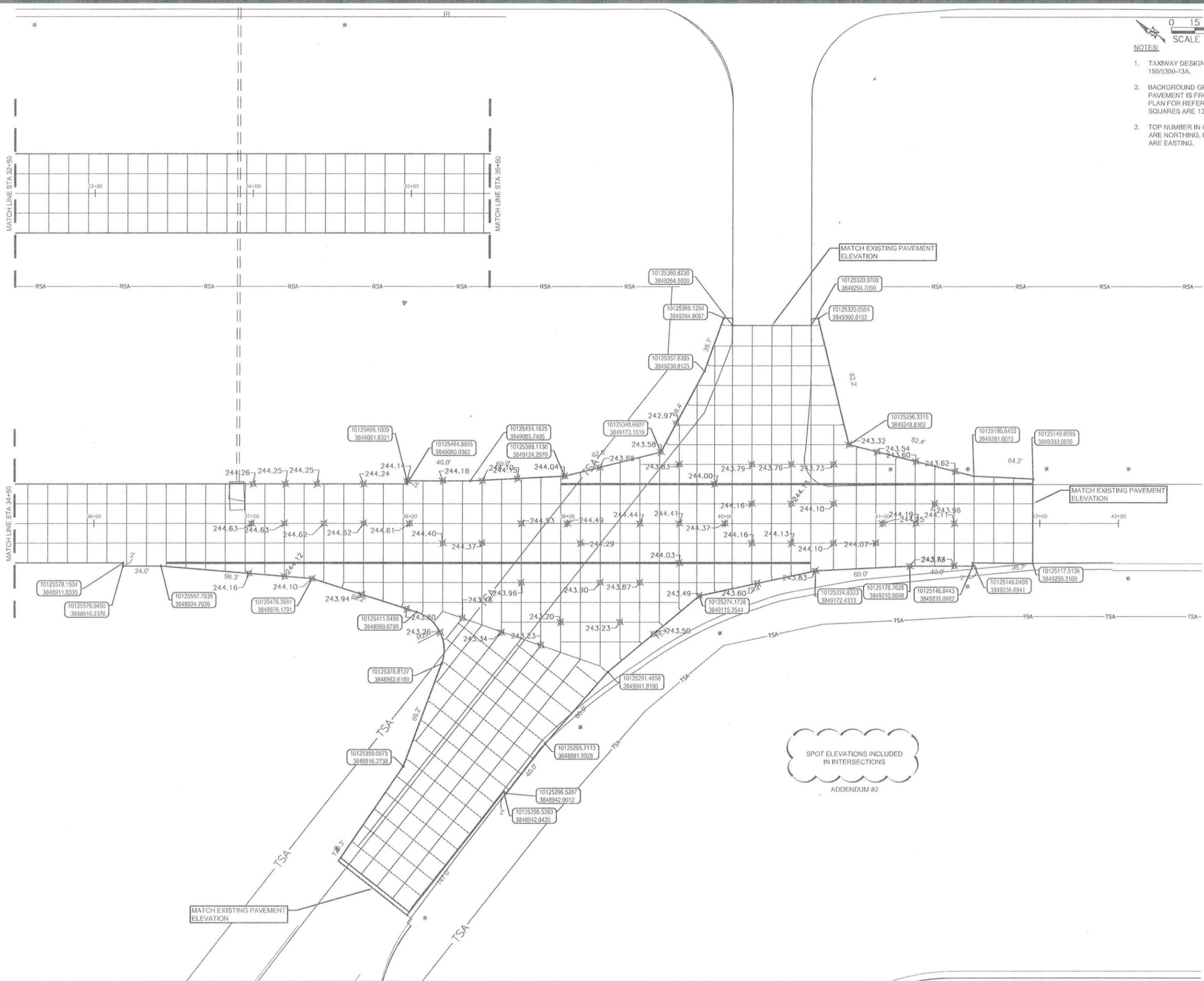
SHEET NO.

C22



SPOT ELEVATIONS INCLUDED
 IN INTERSECTIONS
 ADDENDUM #2

FILENAME: X:\CLIENTS\MONTGOMERY COUNTY\930101.00-AIRPORT-THEX\DRAWINGS\CO-PLANS\C21-C24-DIMENSION CONTROL-PLAN.DWG PLOT DATE: 8/4/2017 10:09 AM PLOT STYLE: SPLOD.DWTB



- NOTES:
1. TAXIWAY DESIGN IS TDG 3 PER AC 150/5300-13A.
 2. BACKGROUND GRID FOR PROPOSED PAVEMENT IS FROM PAVING JOINT PLAN FOR REFERENCE. TYPICAL GRID SQUARES ARE 12.5' X 12.5'.
 3. TOP NUMBER IN COORDINATE LABELS ARE NORTHING, BOTTOM NUMBERS ARE EASTING.

SPI
 SCHAUMBURG & POLK, INC.
 BEAUMONT • HOUSTON • TYLER

ADDRESS:
 320 S. Broadway,
 Suite 200
 Tyler, TX 75702

TELEPHONE:
 903-595-3913

FAX:
 903-595-2093

Firm Regist. #: F000520
 © Copyright 2017



PROJECT NAME:
CONROE NORTH HOUSTON REGIONAL AIRPORT TAXIWAY EXTENSION

1812CONRO

SHEET NAME:
ALTERNATE BID: DIMENSION CONTROL PLAN

STA 32+50 TO STA 41+95.55

DATE: 07/13/2017
REVISION 1: ADDENDUM #2 AUGUST 7th, 2017
REVISION 2: --
REVISION 3: --
REVISION 4: --
REVISION 5: --
DRAWN BY: T. REED
REVIEWED BY: F. STEPHENS
PROJECT NO. 930101.00
SHEET NO. C24