



3000 Internet Blvd.  
Suite 400  
Frisco, TX 75034  
TEL 972.377.7480  
FAX 972.377.8380  
[www.GarverUSA.com](http://www.GarverUSA.com)

**August 11, 2023**

**2318ADDSN - Addendum No. 1  
To Plans, Contract Documents and Specifications**

**Addison Municipal Airport (ADS)  
Taxiway B Extension – Phase I  
TxDOT CSJ No. 2318ADDSN**

This addendum shall be a part of the Plans, Contract Documents and Specifications to the same extent as though it were originally included therein, and it shall supersede anything contained in the Plans, Contract Documents and Specifications with which it might conflict. Acknowledgement of receipt of this Addendum must be provided on TxDOT Bid Form page 11 of 12 included in the Contract Documents.

**The pre-bid meeting minutes have been included along with the sign-in sheet and meeting exhibits. The pre-bid meeting minutes include all questions asked during the pre-bid and the pre-bid site visit.**

**Bidders can obtain the revised bid form Addendum No. 1 on the TxDOT Website 'Plans Online' at <https://planuser.txdotplans@ftp.txdot.gov/plans/Airport/2023/08%20August/Addison%20Municipal%20Airport/2318ADDSN/>**

**Bidders must fill out the bid form electronically, print, sign and submit a hardcopy as part of their bid package.**

**Revisions or additions made to the Contract Documents and Plans:**

**Plans**

Sheet CC-502: The scale of the TxDOT detail was adjusted to properly fit the page.

Sheet CC-503: The scale of the TxDOT detail was adjusted to properly fit the page.

Sheet CC-504: The scale of the TxDOT detail was adjusted to properly fit the page.

Sheet EL-101: Sign legends were corrected to match the sign index.

Sheet CC-501-2: The hatch patterns of Detail 1 were corrected to accurately reflect the called-out materials.

**TxDOT Bid Form for 2318ADDSN**

The revised bid form that was uploaded to the TxDOT Website 'Plans Online' was updated to include the allowances for following bid items:

**Base Bid**

- SS-300-5.5 – ALCMS Control System Modifications and Equipment Updates = \$45,000
- SS-302-3.1 – Power Utility Allowance = \$10,000

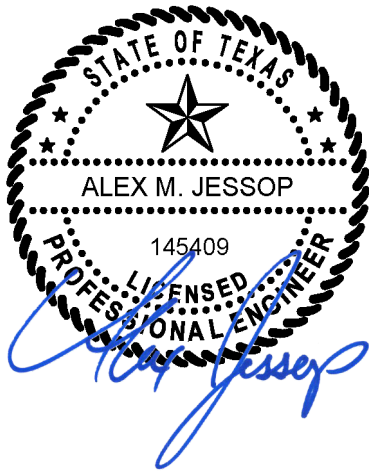
By: \_\_\_\_\_

Alex Jessop, P.E.  
Project Engineer

August 11, 2023  
Addendum No.1

**Attachments:**

Pre-Bid Meeting Minutes  
Revised Plan Sheet CC-502  
Revised Plan Sheet CC-503  
Revised Plan Sheet CC-504  
Revised Plan Sheet EL-101  
Revised Plan Sheet CC-501-2



Digitally Signed 08/11/2023

# PREBID MEETING MINUTES





3000 Internet Blvd.  
Suite 400  
Frisco, TX 75034  
TEL 972.377.7480  
FAX 972.377.8380  
[www.GarverUSA.com](http://www.GarverUSA.com)

## MEETING MINUTES

**To:** Attendees  
**From:** Sara Andrews, PE  
**RE:** Addison Airport (ADS)  
TxDOT CSJ No. 2318ADDSN  
Taxiway Bravo Extension Phase I – Pre-Bid Meeting

**Date:** August 11, 2023

**Copies To:** Attendees

On Wednesday, August 9, 2023, at 9:00am, a Pre-Bid meeting was conducted to discuss the Taxiway Bravo Extension Phase I project. The following items were discussed:

**1) Sign In Sheet Attached**

- a) See attached Sign-In Sheet.

**2) Bidding Procedures**

- a) Sealed bids need to be addressed and delivered to Mr. Wil Newcomer, Purchasing Manager for the Town of Addison Finance Dept., 5350 Belt Line Road, Dallas, TX 75254.
- b) Bids will be received until 2:00pm, Wednesday August 30, 2023, then publicly opened and read.
  - i. Any bids received after closing time will be returned unopened.
  - ii. The Bid Opening for this project will be held in the Town of Addison Finance Department at the address listed for sealed bid delivery.
- c) Technical questions concerning the plans and specifications should be directed to Sara Andrews, PE at [SCAndrews@garverusa.com](mailto:SCAndrews@garverusa.com).
- d) The deadline for questions is 5:00pm on Friday August 18, 2023. Answers will be provided by 5:00pm on Wednesday, August 23, 2023.
- e) Bidder Qualifications
  - i. If Contractor is Pre-Qualified with the Texas Department of Transportation, the Contractor shall include the necessary Full Prequalification and Bidder's Questionnaire with the Bid Proposal.
  - ii. If Contractor is not Pre-Qualified, the Contractor shall include the bidder's qualifications per General Provision 20-02, Pre-qualification of Bidder along with the Bid Proposal.

**2) Federal Provisions**

- a) The DBE goal is 10.00%.
  - i. Questions regarding goals and Good Faith Efforts should be directed to Eli Lopez with TxDOT Aviation at (512)-416-4506, [Eli.Lopez@txdot.gov](mailto:Eli.Lopez@txdot.gov).
  - ii. The Bidder must submit an acceptable DBE plan and commitment or good faith effort not later than 5 calendar days after bid opening (***Due to the Labor Day Holiday this deadline will be Tuesday September 5, 2023 by 5:00pm***) to Eli Lopez via email as a matter of responsibility, even if the bidder is not the apparent low bidder.
- b) Contractor shall follow all Davis Bacon Wage Rate Requirements.

- c) Contractor shall follow all Buy American clauses.

**3) Instructions to bidders**

- a) Bid Proposal
- i. Contract Time: 270 Calendar Days (Base Bid + any combination of Additive Alternates)
  - ii. \$2,300 Liquidated Damages per day
  - iii. Bid proposals need to be completed electronically via Adobe Reader.
  - iv. Once bid proposals are completed via Adobe Reader, Contractor will need to sign the proposal in ink.
  - v. All addendums need to be acknowledged on the Bid Proposal. Addendums will be posted online through TxDOT.
- b) TxDOT General Provisions
- i. General Provisions are provided in a standalone publication entitled General Provisions.
  - ii. Electronic copies are available on the TxDOT Aviation website.
    - a. <https://www.txdot.gov/inside-txdot/division/aviation/general-provisions.html>
  - iii. Contractors shall pay close attention to Section 100 in the General Provisions regarding the Contractor Quality Control Program and the Contractor Quality Control Testing.
    - a. The Contractor is required to prepare a Quality Control Program following the specifications where it is required.
    - b. Owner will perform Quality Assurance (QA) testing on all materials.
  - iv. Any failed tests performed by the QA lab will be deducted from the Contractor.

**4) Construction Plans**

- a) The attached exhibits were discussed during the Pre-Bid meeting.
- b) The plans are separated out per Base Bid and Additive Alternates.
- c) The Additive Alternates will be awarded in the order they are numbered.

**5) Construction Specifications**

**3) Pre-Bid Meeting and Site Visit Questions**

- a) Will the hangars north of Phase 4 limits remain accessible to active aircraft during Phase 4 construction?
- The contractor must maintain access for active aircraft taxiing to these hangars during Phase 4 construction. Tenant access will be coordinated with the airport prior to Phase 4 construction.
- b) In which phase would Additive Alternate 4 be included within?
- The Runway Weather Information System (RWIS) of Additive Alternate 4 is to be included within the four weekend Runway closures of Phase 2 and Phase 3A, if awarded.
- c) Will the contractor have access to a water source?
- Yes, the contractor can utilize the fire hydrants within the vicinity of the designated staging area. The contractor will be required to obtain a meter from the Town of Addison prior to accessing the hydrants.

TxDOT CSJ No. 2318ADDSN  
ADS – Taxiway Bravo Extension Phase I  
Pre-Bid Meeting Minutes

- d) What is the anticipated start date for this project?
  - The project is anticipated to begin as early as October or early November and finishing up in July of 2024 currently.
- e) Sheets 38 – 40 did not plot correctly, can you reissue these?
  - Yes, these sheets will be corrected and reissued in Addendum No. 1.
- f) The hatch pattern for the Concrete Encased Pipe detail on plan sheet 162 is for CTB. The note says 6" Lime treated subgrade. Can you confirm which is required?
  - The note was correct, the hatch patterns have been updated, and the updated sheet will be included in Addendum No. 1.
- g) Additive Alternate 2, Sheet 168, shows the joint layout for the VSR. Except for the tie-ins, there is no reinforcement in this item. The bid item is for TxDot Item 360. Is it the intent to use item 360 for the payment and the joint layout for the reinforcement?
  - An answer will be provided in Addendum No. 2.
- h) What is the ratio of escorts?
  - The contractor shall escort the number of personnel they can safely account for on the airfield. The RPR is not an escort for the contractor.
- i) Is there a cost for the badging?
  - The Airport administers the training and badging at no cost to the contractor.
- j) Will the owner be setting the values for the allowance items?
  - The allowance values will be updated in the attached Bid Form of Addendum No. 1.
- k) Is eVerify required by TxDOT for this project?
  - No, TxDOT Aviation does not require the contractor completes the eVerify process for aviation projects.
- l) What lab is the engineer using for QA?
  - STL Engineers will be responsible for QA testing on this project.
- m) Can spoils be left on site?
  - Spoils may be stockpiled on site in the designated stockpile area per the plans.
- n) Can a batch plant be brought onsite?
  - A batch plant may be brought onsite; however, it will need to be coordinated with the airport, engineer, and FAA through an airspace analysis prior to the start of construction.
- o) Would a geogrid on the TxDOT preapproved list be acceptable over a Triaxial geogrid?
  - An answer will be provided in Addendum No. 2.
- p) Will a field office for the RPR be required of the contractor?
  - An office for the RPR is not required by the contractor.
- q) Will the contractor be required to supply radios for the RPR?
  - No.

TxDOT CSJ No. 2318ADDSN  
ADS – Taxiway Bravo Extension Phase I  
Pre-Bid Meeting Minutes

- r) Will the contractor need to comply with the Buy American Act and the Build America By America Act requirements?
  - Yes. The contractor shall contact TxDOT with any additional Buy American Act inquiries.
- s) Will the PAPI's need to remain operation between weekend closures?
  - The PAPIs can remain out of service during the time between the four-weekend runway closures but must be operational after the final weekend closure. The airport will issue a NOTAM for the PAPIs while they are non-operational.
- t) Would it be possible to get a list of which signs appear on what bid schedule?
  - Signs 98, 99, 100, and 101 are components of Additive Alternate 1. All other signs are a component of the base bid.
- u) Three signs (no's 62, 69, 95) on the sign schedule are listed as size 2 while all the rest are size 3. Would you please confirm if that is correct?
  - The three size 2 signs (62, 69, and 95) are that way due to the proximity of the VSR road. Size 2 signs can be closer to Taxiway A and further away from the VSR versus immediately adjacent to the VSR that would be needed for Sign 3 signs.

**Attachments:** Project Exhibits  
Sign-In

**Pages(incl.):** 10

**Copy to File:** 22A11112

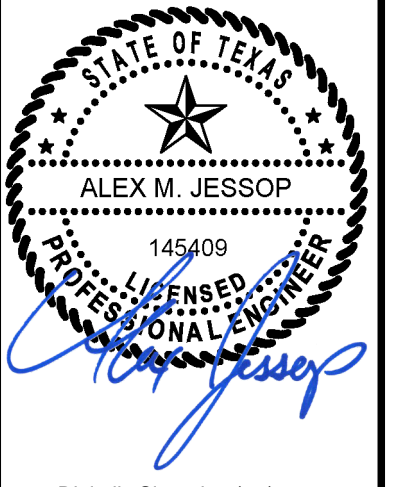
**Copies to Garver:** SCA, AMJ

L:\2022\22A11112 - ADS Taxiway Bravo Extension Phase I\Correspondence\Meetings\Minutes\2023-08-09 Pre bid Meeting\2318ADDSN - ADS Taxiway Bravo Extension Phase 1 - PreBid Meeting Minutes.docm

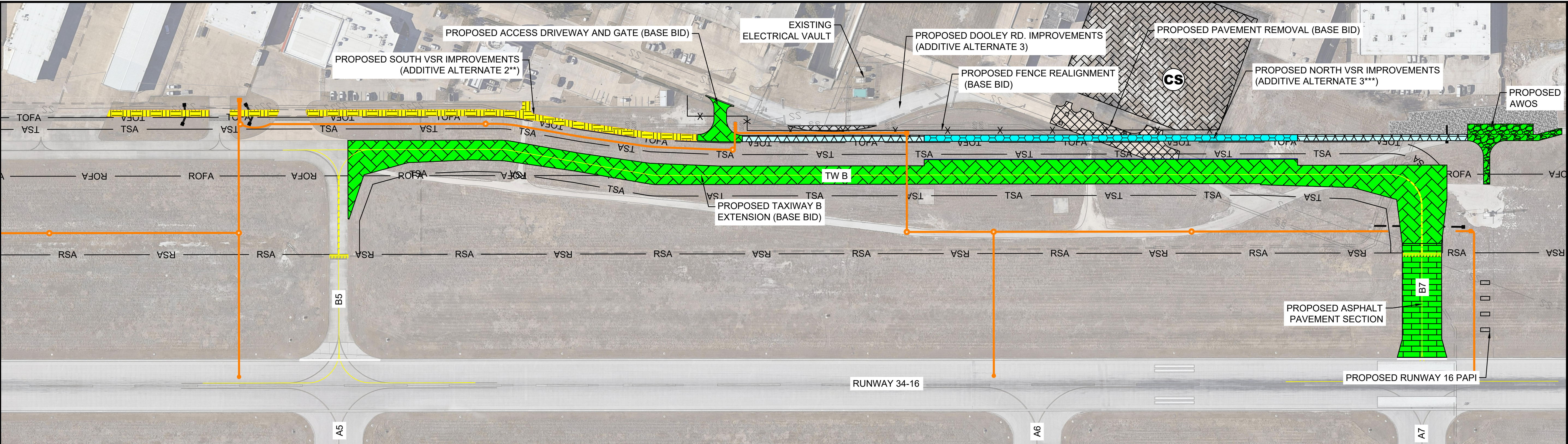


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**TAXIWAY B IMPROVEMENTS (BASE BID)**  
**VSR SOUTH EXTENSION (ADDITIVE ALTERNATE 2)**  
**VSR NORTH EXTENSION (ADDITIVE ALTERNATE 3)**  
**RUNWAY WEATHER INFORMATION SYSTEM (ADDITIVE ALTERNATE 4)**

**ITEMS OF WORK**

**BASE BID\*:**  
 CONSTRUCT PROPOSED TAXIWAY B EXTENSION, PROPOSED ACCESS DRIVEWAY, GRAVEL VEHICLE SERVICE ROAD EXTENSION FROM PROPOSED ACCESS DRIVE, AND PROPOSED FENCING WORK, INCLUDING ASSOCIATED EARTHWORK, PAVEMENT SECTION CONSTRUCTION, ELECTRICAL IMPROVEMENTS, DRAINAGE IMPROVEMENTS, AND PAVEMENT MARKINGS.

**ADDITIVE ALTERNATE 1: TAXIWAY B1**  
 CONSTRUCT TAXIWAY B1 EXTENSION, INCLUDING ASSOCIATED EARTHWORK, DEMOLITION, DRAINAGE IMPROVEMENTS, LIGHTING AND SIGNAGE IMPROVEMENTS, AND PAVEMENT MARKING.

**ADDITIVE ALTERNATE 2\*\*:** VSR SOUTH EXTENSION  
 CONSTRUCT SOUTH VEHICLE SERVICE ROAD EXTENSION BETWEEN TAXIWAY B1 AND PROPOSED ACCESS DRIVEWAY, INCLUDING ASSOCIATED EARTHWORK, DEMOLITION, DRAINAGE IMPROVEMENTS, AND ELECTRICAL IMPROVEMENTS.

**ADDITIVE ALTERNATE 3\*\*\*:** VSR NORTH EXTENSION  
 CONSTRUCT NORTH VEHICLE SERVICE ROAD EXTENSION BETWEEN PROPOSED ACCESS DRIVEWAY AND PROPOSED AWOS SITE, INCLUDING ASSOCIATED EARTHWORK, DEMOLITION, DRAINAGE IMPROVEMENTS, AND ELECTRICAL IMPROVEMENTS.

**ADDITIVE ALTERNATE 4\*\*\*\*:** RUNWAY WEATHER INFORMATION SYSTEM (RWIS)  
 CONSTRUCT ELECTRICAL IMPROVEMENTS FOR RUNWAY 34-14 RWIS.

**NOTES:**

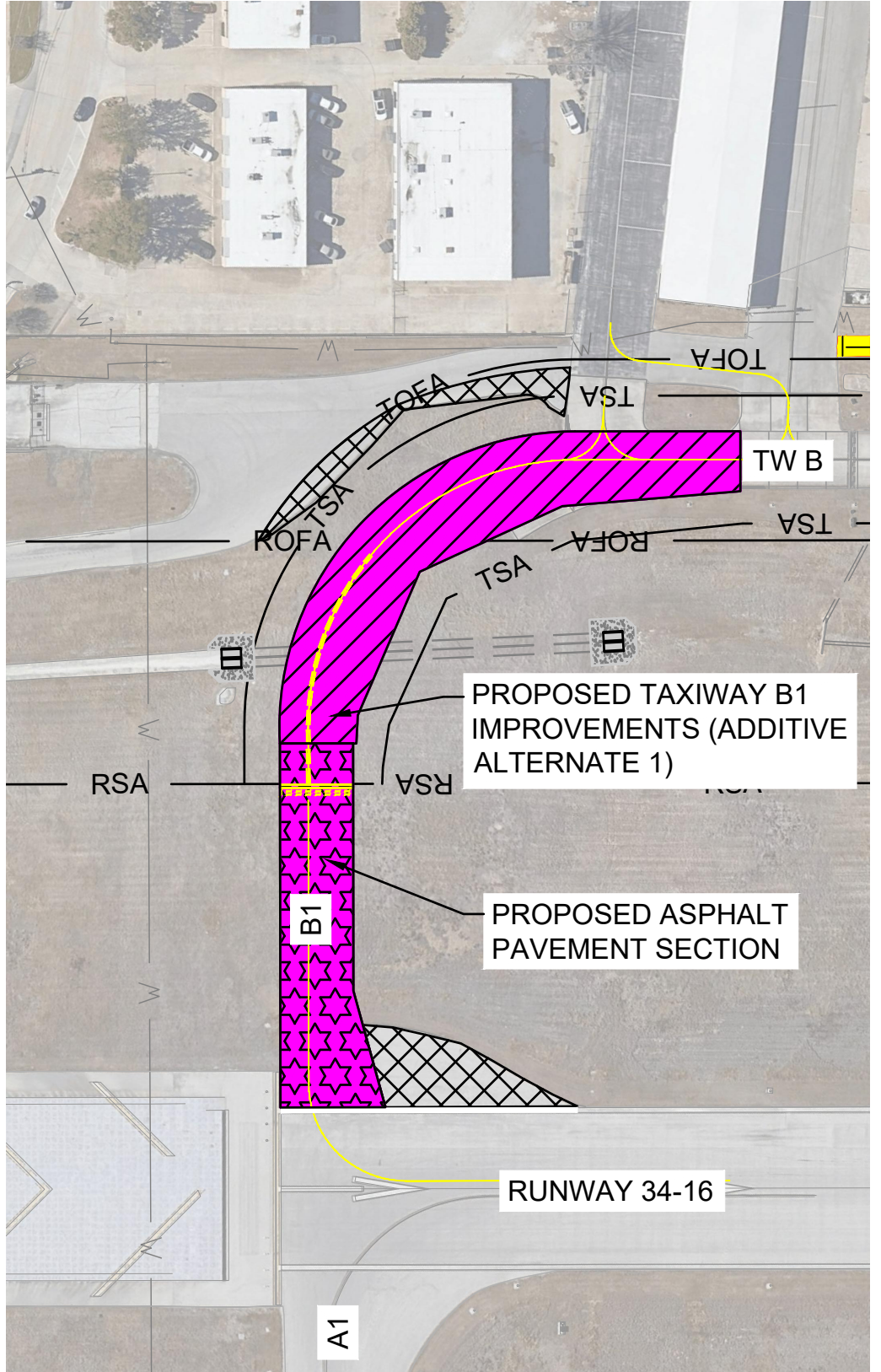
\*IF ADDITIVE ALTERNATE 2 IS NOT AWARDED, THE CONTRACTOR SHALL CONSTRUCT A GRAVEL VEHICLE SERVICE ROAD FROM THE PROPOSED ACCESS DRIVEWAY OFF OF DOOLEY ROAD SOUTH TO THE PROPOSED ATCT DRIVEWAY ENTRANCE.

IF ADDITIVE ALTERNATE 3 IS NOT AWARDED, THE CONTRACTOR SHALL CONSTRUCT A GRAVEL VEHICLE SERVICE ROAD FROM THE PROPOSED ACCESS DRIVEWAY OFF OF DOOLEY ROAD NORTH TO THE PROPOSED AWOS SITE. AND SOUTH TO THE PROPOSED ATCT DRIVEWAY ENTRANCE.

\*\*IF ADDITIVE ALTERNATE 2 IS AWARDED, THE CONTRACTOR WILL CONSTRUCT A CONCRETE VEHICLE SERVICE ROAD IN LIEU OF THE BASE BID GRAVEL VEHICLE SERVICE ROAD FROM THE PROPOSED ACCESS DRIVEWAY OFF OF DOOLEY ROAD TO THE PROPOSED ATCT DRIVEWAY ENTRANCE.

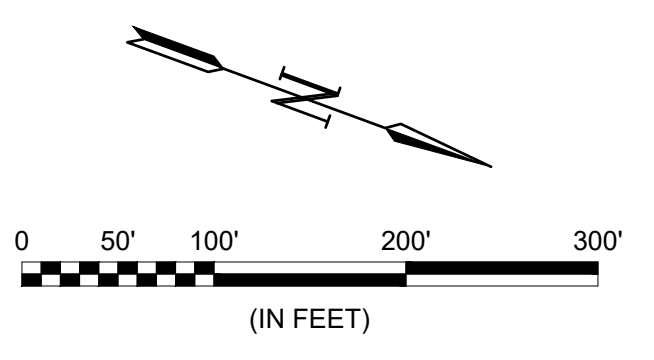
\*\*\*IF ADDITIVE ALTERNATE 3 IS AWARDED, THE CONTRACTOR WILL CONSTRUCT A CONCRETE VEHICLE SERVICE ROAD IN LIEU OF THE BASE BID GRAVEL VEHICLE SERVICE ROAD FROM THE PROPOSED ACCESS DRIVEWAY OFF OF DOOLEY ROAD TO THE PROPOSED AWOS SITE AND TO THE PROPOSED ATCT DRIVEWAY ENTRANCE. THE PORTION OF THE VEHICLE SERVICE ROAD ADJACENT TO THE PAVED SHOULDER WILL REMAIN GRAVEL IF ADDITIVE ALTERNATE 3 IS AWARDED

\*\*\*\*IF ADDITIVE ALTERNATE 4 IS AWARDED, THE CONTRACTOR WILL CONSTRUCT THE RWIS ELECTRICAL IMPROVEMENTS WITHIN THE RUNWAY 34-16 RSA DURING THE RUNWAY CLOSURES OF PHASE 2 & 3A OF CONSTRUCTION.



**TAXIWAY B1 IMPROVEMENTS (ADDITIVE ALTERNATE 1)**

LEGEND			
	PROPOSED CONCRETE PAVEMENT (BASE BID)		VSR SOUTH EXTENSION (ADDITIVE ALTERNATE 2)
	PROPOSED ASPHALT PAVEMENT (BASE BID)		PROPOSED CONCRETE PAVEMENT (ADDITIVE ALTERNATE 3)
	PROPOSED CONCRETE PAVEMENT (ADDITIVE ALTERNATE 1)		PROPOSED GRAVEL PAVEMENT (ADDITIVE ALTERNATE 3)
	PROPOSED ASPHALT PAVEMENT (ADDITIVE ALTERNATE 1)		PROPOSED RWIS (ADDITIVE ALTERNATE 4)
	PAVEMENT DEMOLITION		POTENTIAL CONTRACTOR STAGING LOCATION
	RSA — RUNWAY SAFETY AREA		PROPOSED AOA FENCING
	ROFA — RUNWAY OBJECT FREE AREA		EXISTING SANITARY SEWER LINE
	TSA — TAXIWAY SAFETY AREA		EXISTING STORM DRAIN
	TOFA — TAXIWAY OBJECT FREE AREA		PROPOSED STORM DRAIN



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BY:	
DESCRIPTION:	
DATE:	
REV:	
ADDISON AIRPORT (ADS) ADDISON, TX	ADDISON AIRPORT
TAXIWAY B EXTENSION PHASE I	
PROJECT LAYOUT PLAN 2	
JOB NO.: 22A11112 DATE: JULY 2023 DESIGNED BY: RJT DRAWN BY: RJT	
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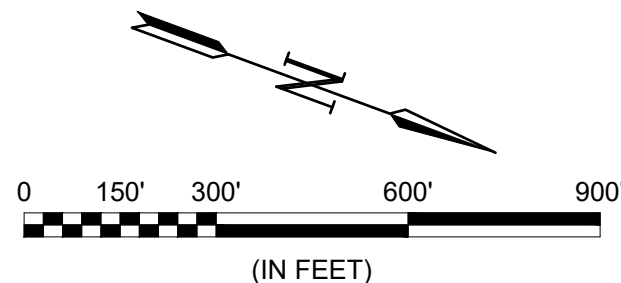
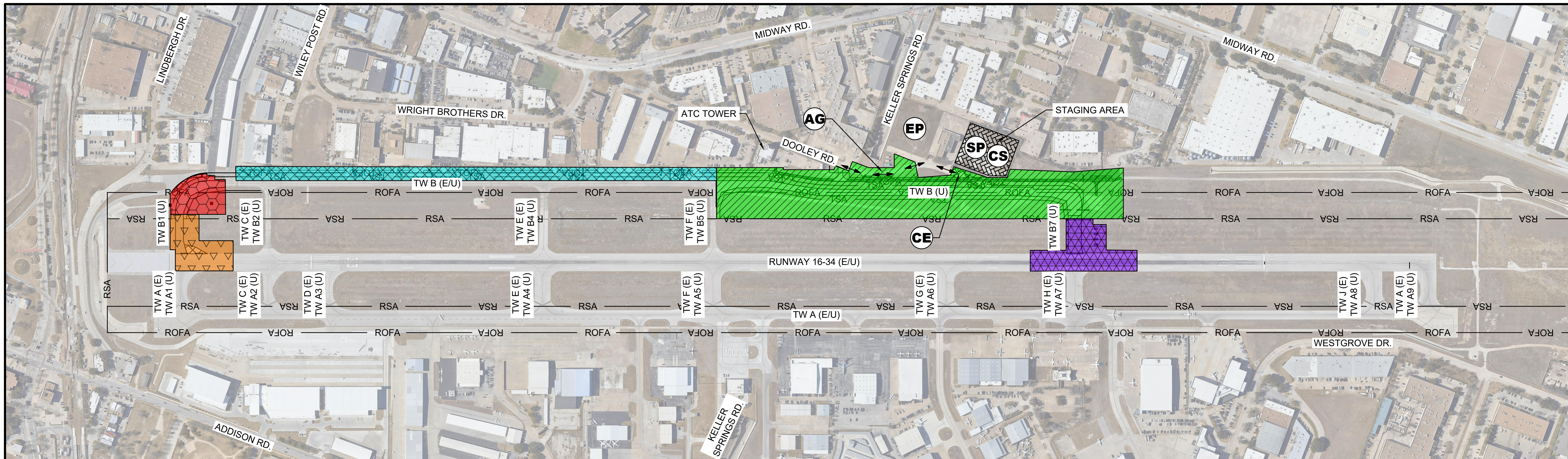


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**LEGEND**

- RSA — RUNWAY SAFETY AREA
- ROFA — RUNWAY OBJECT FREE AREA
- TSA — TAXIWAY SAFETY AREA
- TOFA — TAXIWAY OBJECT FREE AREA
- ▨ CONTRACTOR STAGING AREA
- AG ACCESS GATE
- CE CONSTRUCTION ENTRANCE
- CS CONSTRUCTION STAGING AREA
- SP EXCAVATION STOCKPILE SITE
- ↔ CONTRACTOR ACCESS ROUTE

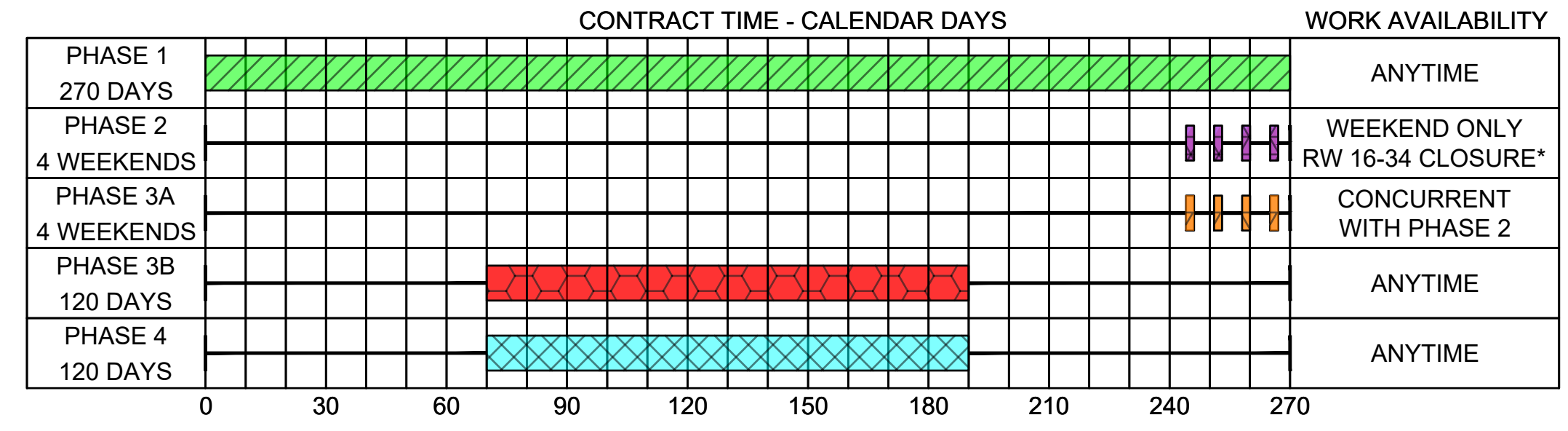
**SEQUENCE OF CONSTRUCTION**

- 1** PERFORM EARTHWORK, DEMOLISH PAVEMENT, EXISTING UTILITY IMPROVEMENT, RELOCATE AND CONSTRUCT AWOS AND SAWS SYSTEMS, CONSTRUCT TAXIWAY B PAVEMENT SECTION, INSTALL FENCE ALONG DOOLEY ROAD, CONSTRUCT VSR PAVEMENT SECTION, CONSTRUCT DOOLEY ROAD ENTRANCE AND GATE, ELECTRICAL IMPROVEMENTS, DRAINAGE IMPROVEMENTS, AND PAVEMENT MARKINGS.
- 2** **CLOSE RUNWAY 16-34**, PERFORM EARTHWORK, DEMOLISH EXISTING PAPI AND PAPI INFRASTRUCTURE, CONSTRUCT TAXIWAY B7 PAVEMENT SECTION, INSTALL ELECTRICAL INFRASTRUCTURE, PLACE CONCRETE FOUNDATIONS AND PROPOSED PAPIs, AND PAVEMENT MARKINGS.
- 3A** **CLOSE RUNWAY 16-34**, PERFORM EARTHWORK, DEMOLISH PAVEMENT, CONSTRUCT TAXIWAY B1 PAVEMENT SECTION, INSTALL ELECTRICAL IMPROVEMENTS, AND PAVEMENT MARKINGS.
- 3B** DEMOLISH PAVEMENT, PERFORM EARTHWORK, CONSTRUCT TAXIWAY B1 PAVEMENT SECTION, INSTALL ELECTRICAL IMPROVEMENTS, DRAINAGE IMPROVEMENTS, AND PAVEMENT MARKINGS.
- 4** PERFORM EARTHWORK, DEMOLISH PAVEMENT, CONSTRUCT VSR PAVEMENT SECTION, ELECTRICAL IMPROVEMENTS, AND DRAINAGE IMPROVEMENTS.

**NOTE:**

1. CONTRACTOR MUST STORE ALL EQUIPMENT AND MATERIALS OUTSIDE RUNWAY OBJECT FREE AREA.
2. CONSTRUCTION VEHICLES, EQUIPMENT, OR MATERIALS MUST NOT BE PARKED OR STORED WITHIN 50' OF BARRICADES OR ACTIVE AIRFIELD PAVEMENT.
3. CONTRACTOR MAY BE IN CLOSE COORDINATION WITH OTHER CONTRACTORS WORKING IN THE FUTURE DEVELOPMENT AREA OFF DOOLEY ROAD.
4. CONTRACTOR SHALL RELOCATE COMMUNICATION LINES AND POWER LINES FOR THE AFFECTED FAA AWOS EQUIPMENT PER THE ELECTRICAL PLANS PRIOR TO ANY SUBGRADE OR PAVING WORK.

**CONTRACT TIME**



\*WEEKEND RUNWAY CLOSURES SHALL BE SCHEDULED WITH THE OWNER'S APPROVAL. CLOSURES WILL BE FROM 8:00 PM ON THURSDAY TO 12:00 PM ON SUNDAY.

NO WORK SHALL TAKE PLACE BETWEEN JUNE 28 AND JULY 8 TO ACCOMMODATE KABOOM TOWN AIR SHOW. CONTRACTOR MUST INFORM THE AIRPORT THAT THE RUNWAY WILL BE USABLE ONE WEEK PRIOR TO THE AIRSHOW.

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REV.	DATE	DESCRIPTION

ADDISON AIRPORT (ADS)  
 ADDISON, TX

ADDISON AIRPORT

TAXIWAY B EXTENSION  
 PHASE I

CSPP - OVERALL

JOB NO.: 22A11112  
 DATE: JULY 2023  
 DESIGNED BY: RJT  
 DRAWN BY: RJT

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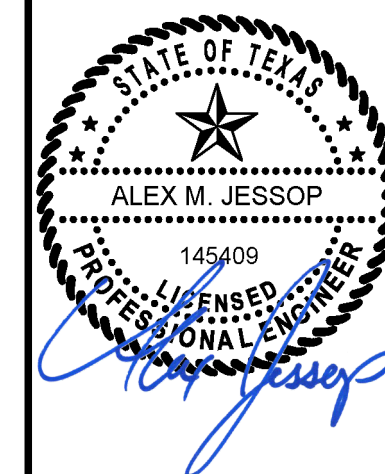
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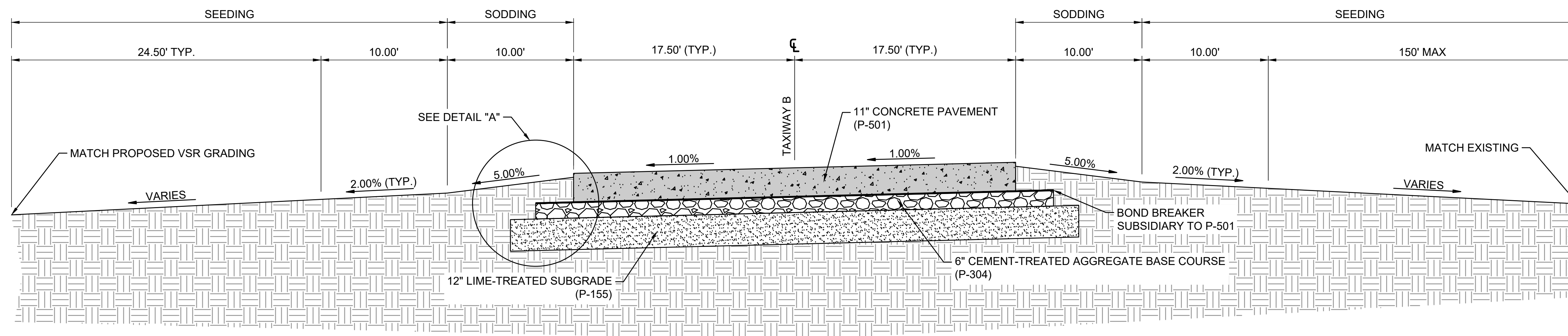


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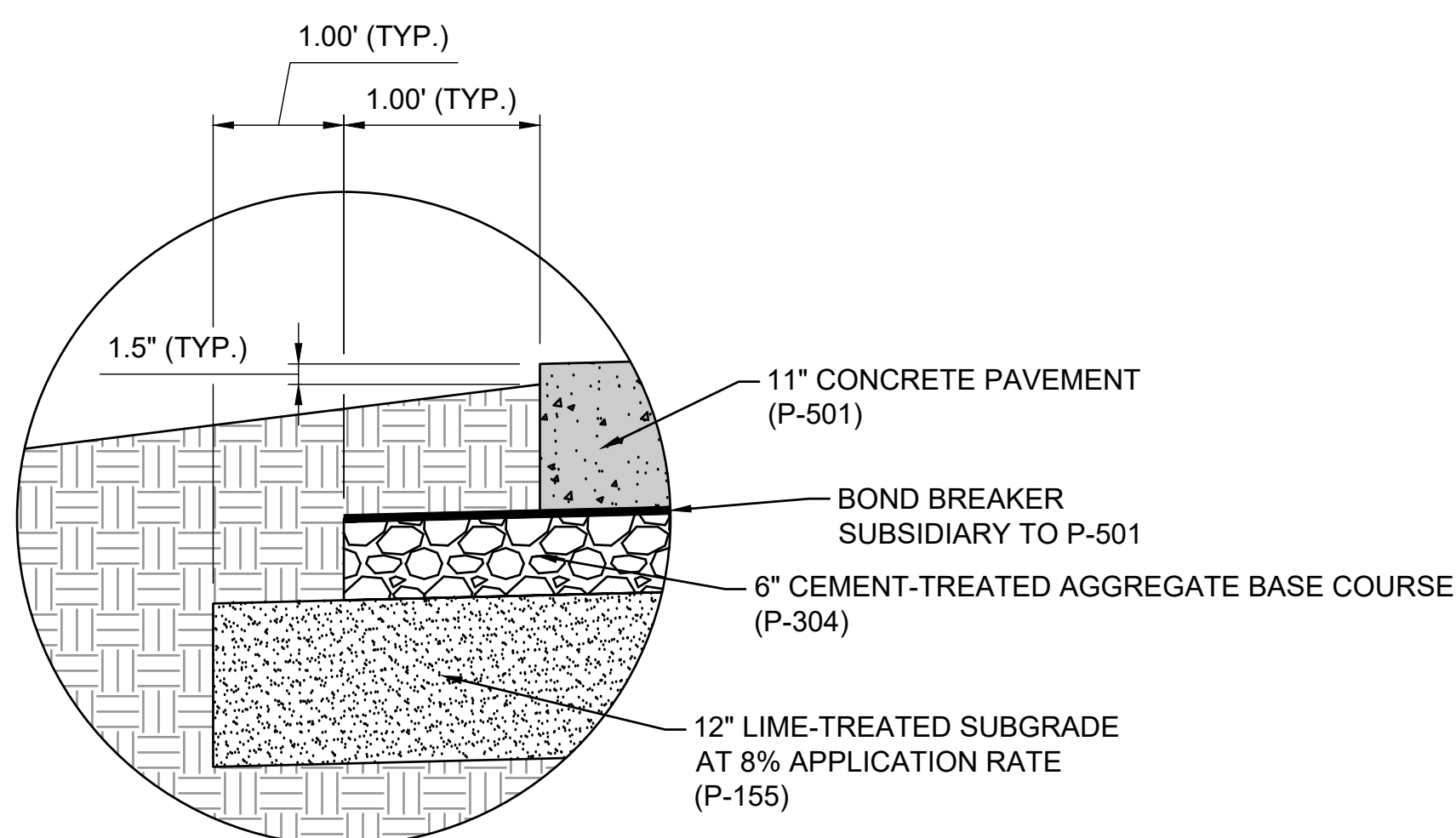
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**1 TAXIWAY B TYPICAL SECTION 1**  
 SCALE: NONE



**A DETAIL A**  
 SCALE: NONE

EARTHWORK SUMMARY PLAN QUANTITIES		
BID SCHEDULE	EXCAVATION	EMBANKMENT
BASE BID	15,200 CY	8,100 CY
ADD. ALT. 1	3,000 CY	600 CY
ADD. ALT. 2	600 CY	1,650 CY
ADD. ALT. 3	500 CY	-

**EARTHWORK QUANTITY NOTES:**

- SEE SPECIFICATION P-152 FOR MATERIAL, METHOD OF MEASUREMENT, AND PAYMENT.
- ALL MATERIAL USED FOR EMBANKMENT MUST MEET THE REQUIREMENTS OF SPECIFICATION P-152.

REV.	DATE	DESCRIPTION	BY

ADDISON AIRPORT (ADS)  
 ADDISON, TX  
 TAXIWAY B EXTENSION  
 PHASE I

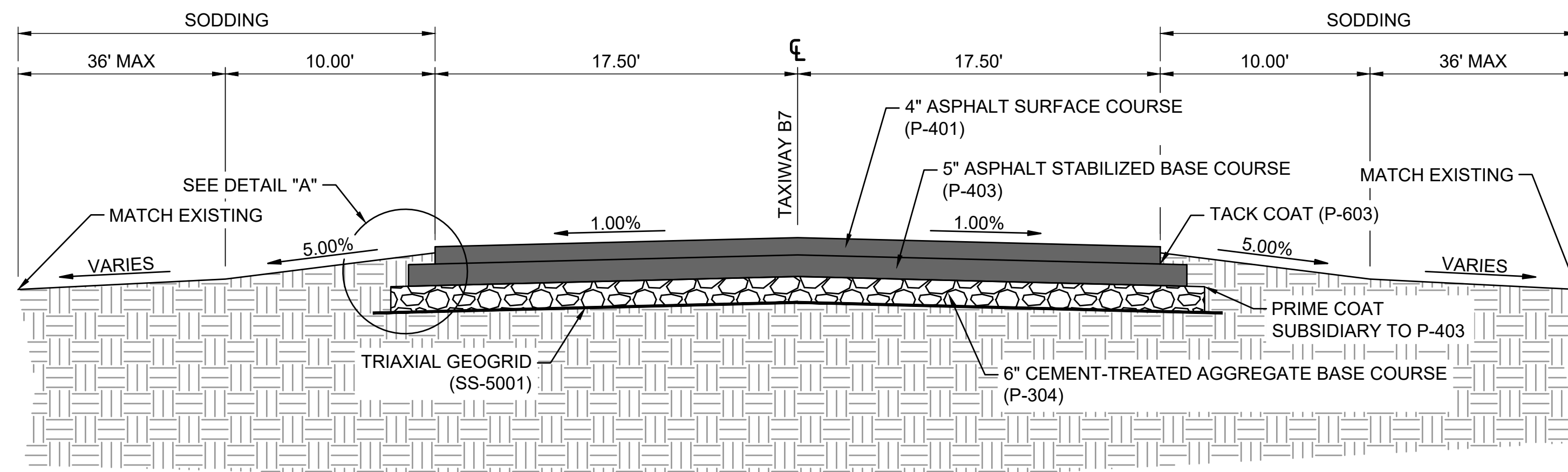
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 DATE: JULY 2023  
 DESIGNED BY: RJT  
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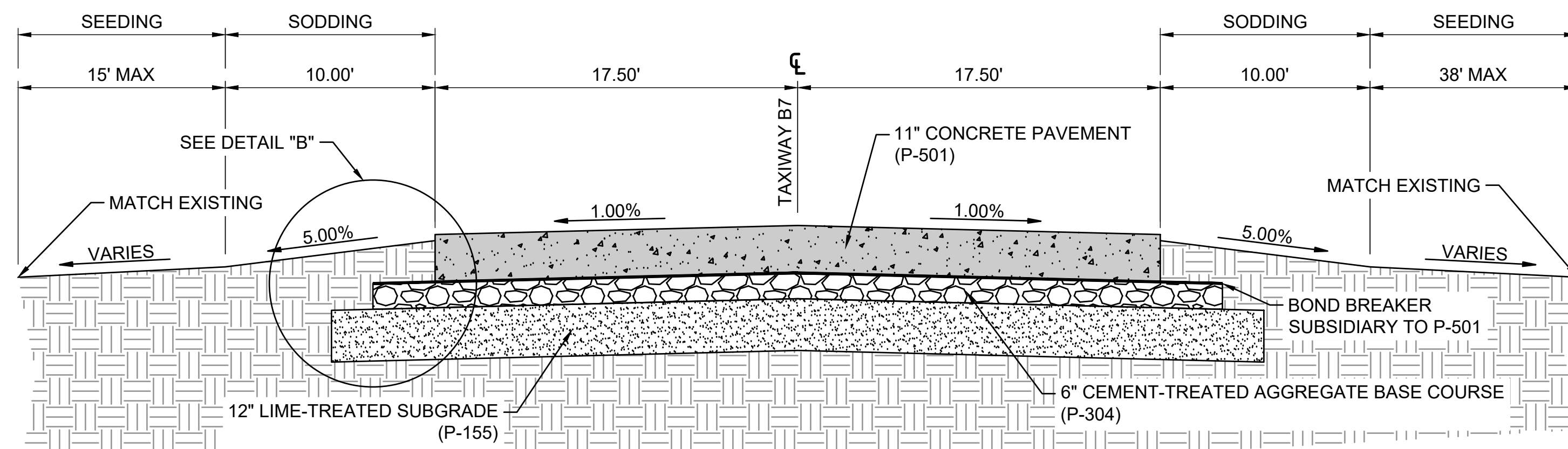
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**42**



**TAXIWAY B7 TYPICAL SECTION 1  
(ACCELERATED PAVEMENT SECTION)**

1  
CP-002

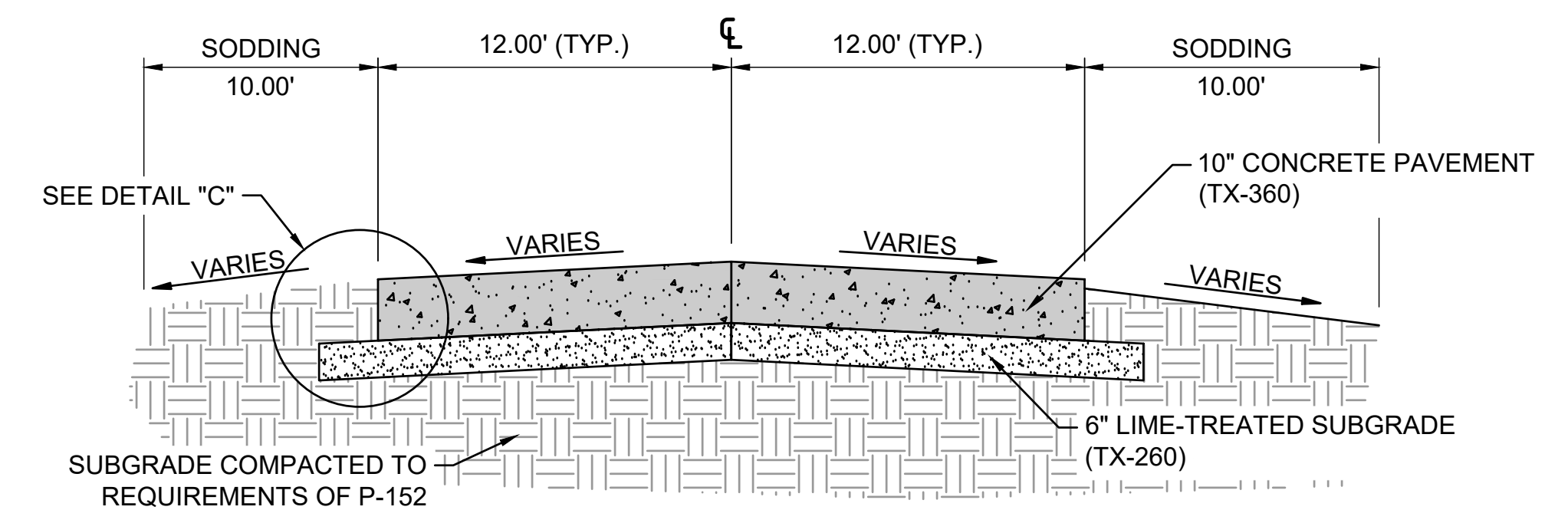
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NOTE:  
ALL DISTURBED AREAS INSIDE THE RSA SHALL BE SODDED.



**TAXIWAY B7 TYPICAL SECTION 2**

2  
CP-002

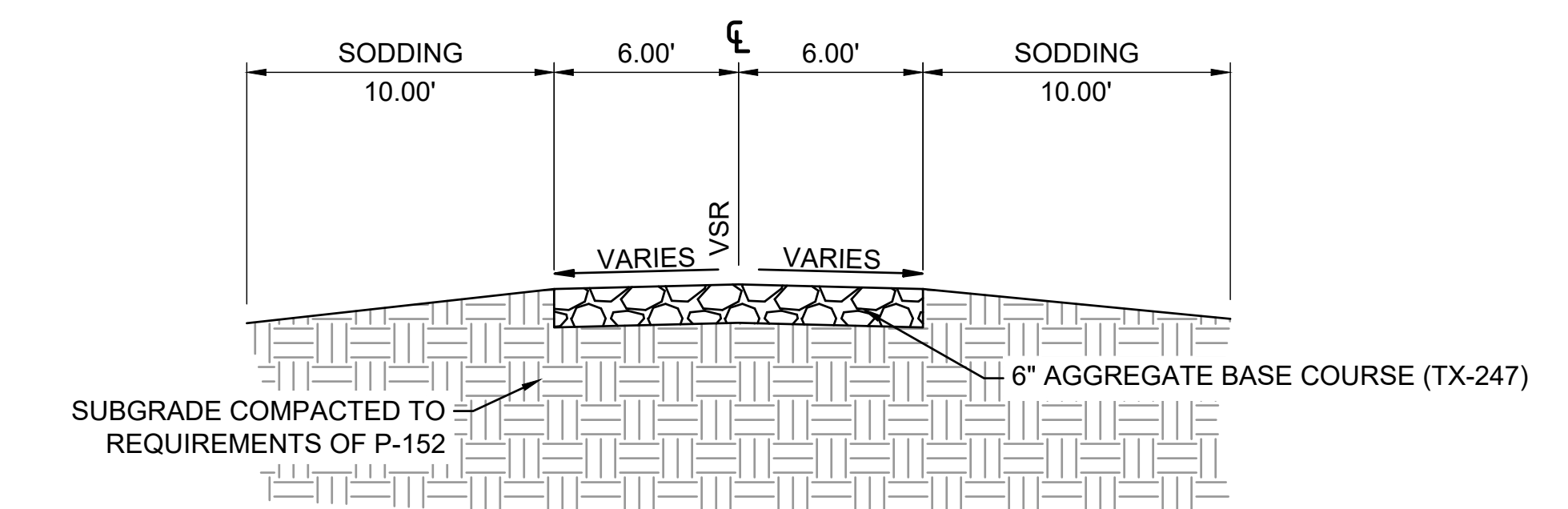
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**ACCESS DRIVEWAY TYPICAL SECTION**

3  
CP-002

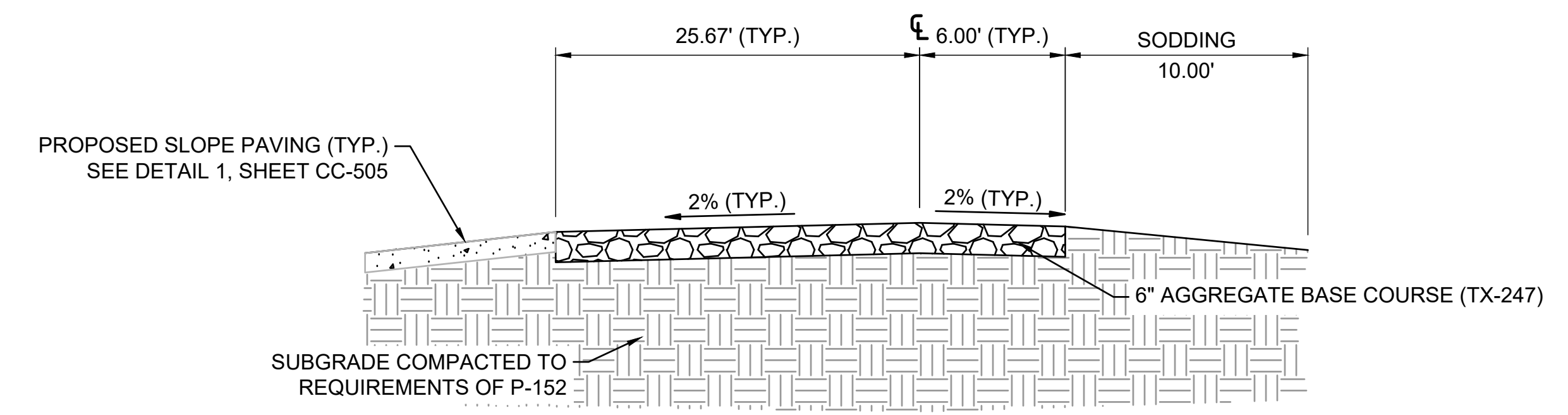
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**VSR TYPICAL SECTION 1**

4  
CP-002

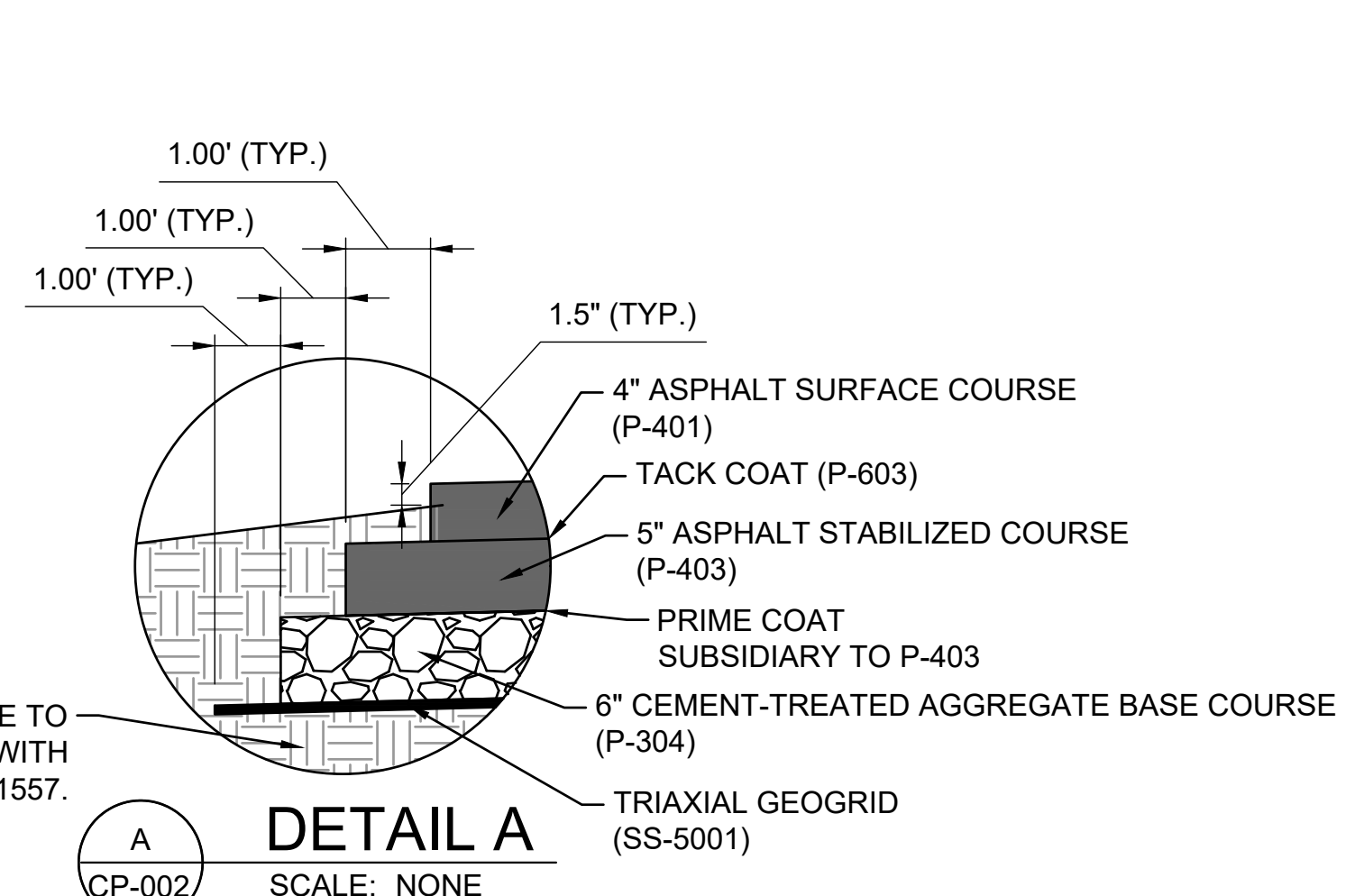
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**AWOS PAD TYPICAL SECTION**

5  
CP-002

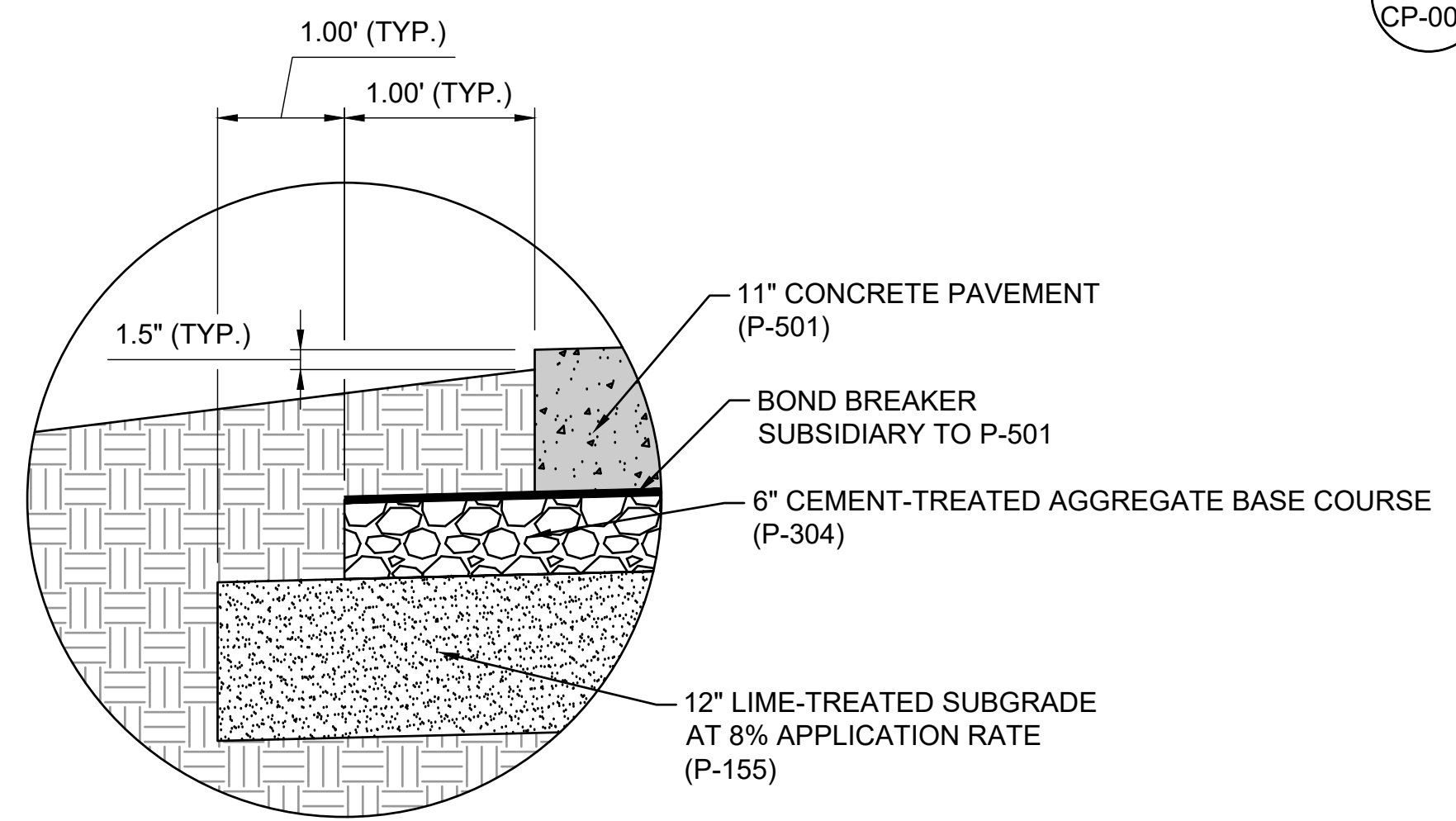
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**DETAIL A**

A  
CP-002

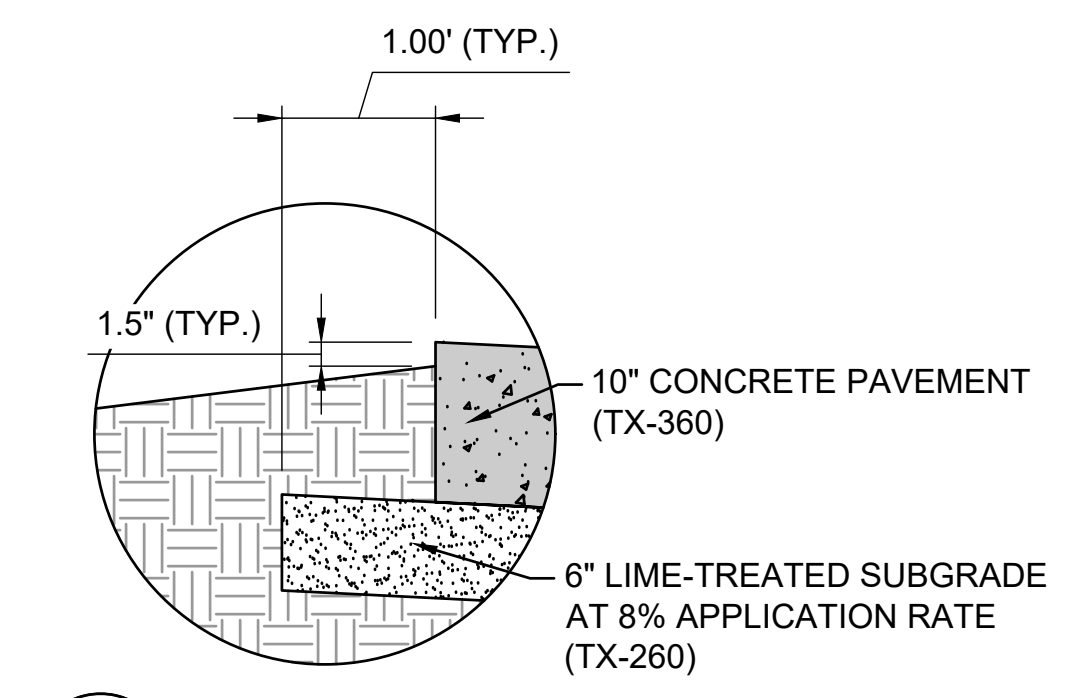
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**DETAIL B**

B  
CP-002

SCALE: NONE



**DETAIL C**

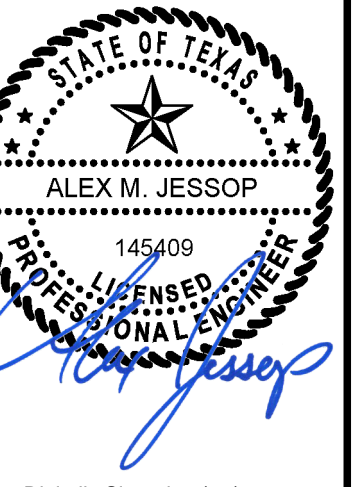
C  
CP-002

SCALE: NONE



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REGISTRATION NO.  
F-5713



Digitally Signed 07/13/2023

REV.	DATE	DESCRIPTION	BY

ADDITION AIRPORT (ADS)  
ADDITION, TX  
ADDITION AIRPORT  
TAXIWAY B EXTENSION  
PHASE I

TYPICAL SECTIONS -  
TAXIWAY B7 & VSR

JOB NO.: 22A11112  
DATE: JULY 2023  
DESIGNED BY: RJT  
DRAWN BY: RJT

BAR IS ONE INCH ON  
ORIGINAL DRAWING  
IF NOT ONE INCH ON THIS SHEET,  
ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**CP-002**

SHEET NUMBER  
**43**

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### SIGN-IN SHEET

Name	Representing	Phone #	Email
John Simmons	Royal Electric	(916) 384-8534	JohnS@royalelect.com
Clay Lassen	Air Side	903-926-4013	lassen@air-sidedolutionsinc.com
Ryan Hindman	TxDOT	(512) 520-7467	ryan.hindman@txdot.gov
Jen Jackson	Zachry	682-2098306	jen.jackson@zachrycorp.com
Melissa Graver	ACP	214-636-4438	migraver@acpconstructors.com
Rick Howard	Webber	346-224-6291	rhoward@webber.com
Chris Johnson	McMahon Contracting	(972) 263-6907	estimating@mcMahoncontracting.com
JOEL JENKINSON	ADDISON AIRPORT	972-392-4855	jjenkinson@addison.tx.gov
SARA ANDREWS	GARVER	972-377-7480	SANDREWS@GARVERUSA.COM
Caleb Chalmers	RPM Construction	214-572-1658	cchalmers@rpmconstruction.com
Stan Allen	Ed Bell Constr.	214-358-6581	Estimating@EdBellConstruction.com



Addison Airport (ADS) – Taxiway Bravo Extension Phase I (TxDOT CSJ No. 2318ADDSN)  
 4545 Jimmy Doolittle Dr, Addison, TX 75001

Pre-Bid Meeting  
 August 9, 2023 9:00am

### SIGN-IN SHEET

Name	Representing	Phone #	Email
Sean Newton	TXDOT	512 694 1767	SEAN.NEWTON@TXDOT.GOV
Matt Bradley	TOPOgraphic	817-891-0242	Matthew.bradley@topographic.com
GREG BRINER	RPMX	817-905-6135	gbriner@RPMXCONSTRUCTION.COM
ALEX JESSOP	GARVER	214-451-2963	AMJESSOP@GARVERUSA.COM
Eli Lopez (Virtual)	TXDOT	512-416-4506	Eli.Lopez@txdot.gov
Wil Newcomer (Virtual)	Town of Addison	972-450-7091	wnewcomer@addisontx.gov

# REVISED PLAN SHEETS

## ADDENDUM NO. 1



BY	AMJ
DESCRIPTION	ADDENDUM NO. 1
DATE	8/10/23
REV.	1

ADDISON AIRPORT (ADS)  
ADDISON, TX

ADDISON AIRPORT

TAXIWAY B EXTENSION  
PHASE I

STORM DRAIN  
DETAILS 2

JOB NO.: 22A11112  
DATE: JULY 2023  
DESIGNED BY: RJT  
DRAWN BY: RJT

BAR IS ONE INCH ON ORIGINAL DRAWING  
IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**CC-502**

SHEET NUMBER  
**38**

**WING DIMENSION CALCULATIONS:**

$$Hw = H + T + C - 0.250'$$

$$Lw = (Hw - 0.250') (SL)$$

For cast-in-place culverts:

$$Atw = (N) (S) + (N + 1) (U)$$

For precast culverts:

$$Atw = (N) (2U + S) + (N - 1) (0.500')$$

$$\text{Total Wingwall Area (SF)} = (0.5) (Hw + 0.250') (Lw) (N - 1)$$

$$\text{Total Concrete Volume (CY)} = [(Wingwall Area) (0.583') + (Lw) (Atw) (0.583') + (Atw) (1.000') (1.167' - 0.583')] \div (27)$$

$$\text{Total Reinforcing (Lb)} = (1.55) (Lw) (Atw) + (4.43) (Atw) + (K) (Hw) (N + 1) (\sqrt{Lw})$$

C = Height of curb above top of top slab (feet)

Hw = Height of wingwall (feet)

K = Constant value for use in formulas

Slope SL:1 K

6:1 ~ 10:41

Atw = Anchor toewall length (feet)

Lw = Length of wingwall (feet)

N = Number of culvert barrels

SL:1 = Side slope ratio (horizontal : 1 vertical)

See applicable box culvert standard for H, S, T, and U values.

**MATERIAL NOTES:**

Provide Grade 60 reinforcing steel.  
Provide galvanized reinforcing steel if required elsewhere in the plans. Adjust reinforcing as necessary to provide a minimum clear cover of 1 1/2".

Provide Class "C" concrete (f'c = 3,600 psi).  
Provide pipe runners, cross pipes, and anchor pipes meeting the requirements of ASTM A53 (Type E or S, Gr B), ASTM A500 Gr B, or API 5LX52.

Provide ASTM A307 bolts.  
Galvanize all steel components, except the concrete reinforcing, unless required elsewhere in the plans, after fabrication.

Repair galvanizing damaged during transport or construction in accordance with Item 445, "Galvanizing."

**GENERAL NOTES:**

Designed according to AASHTO LRFD Bridge Design Specifications.  
The safety end treatments shown herein are intended for use in those installations where out of control vehicles are likely to traverse the openings approximately perpendicular to the cross pipes.

Cross pipes are designed for a traversing load of 10,000 pounds at yield as recommended by Research Report 280-2F, "Safety Treatment of Roadside Parallel-Drainage Structures", Texas Transportation Institute, March 1981.

The quantities for concrete, reinforcing steel, and cross pipes resulting from the formulas given herein are for Contractor's information only.

See the Box Culvert Supplement (BCS) standard sheet for additional dimensions and information.

Alternate design drawings bearing the seal of a professional engineer will be acceptable for precast construction of the safety end treatments.

Cover dimensions are clear dimensions, unless noted otherwise.  
Reinforcing dimensions are out-to-out of bars.

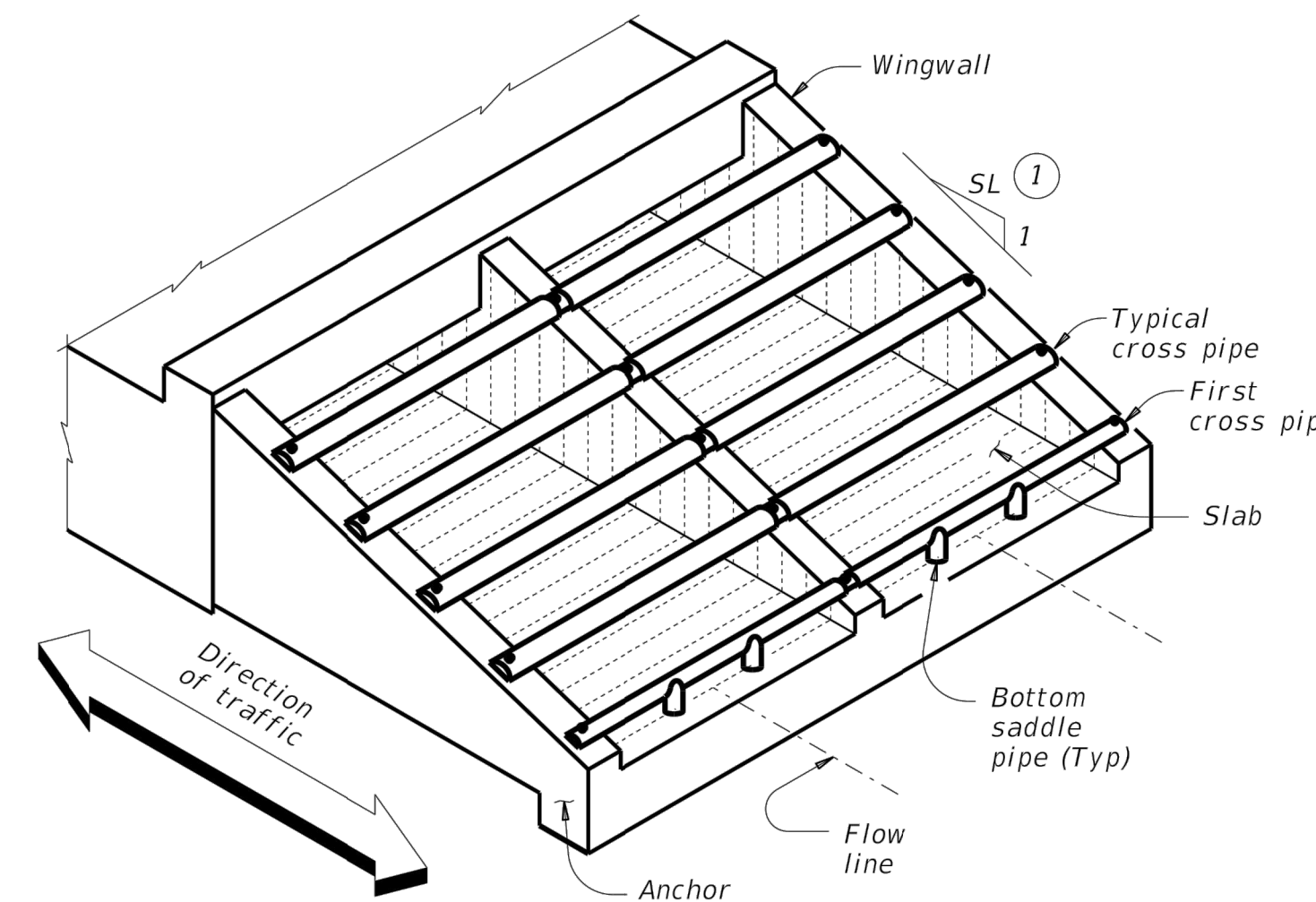
SHEET 1 OF 2

Texas Department of Transportation  
Bridge Division Standard

**SAFETY END TREATMENT  
FOR BOX CULVERTS  
(MAXIMUM Hw = 7'-0")  
TYPE I ~ PARALLEL DRAINAGE**

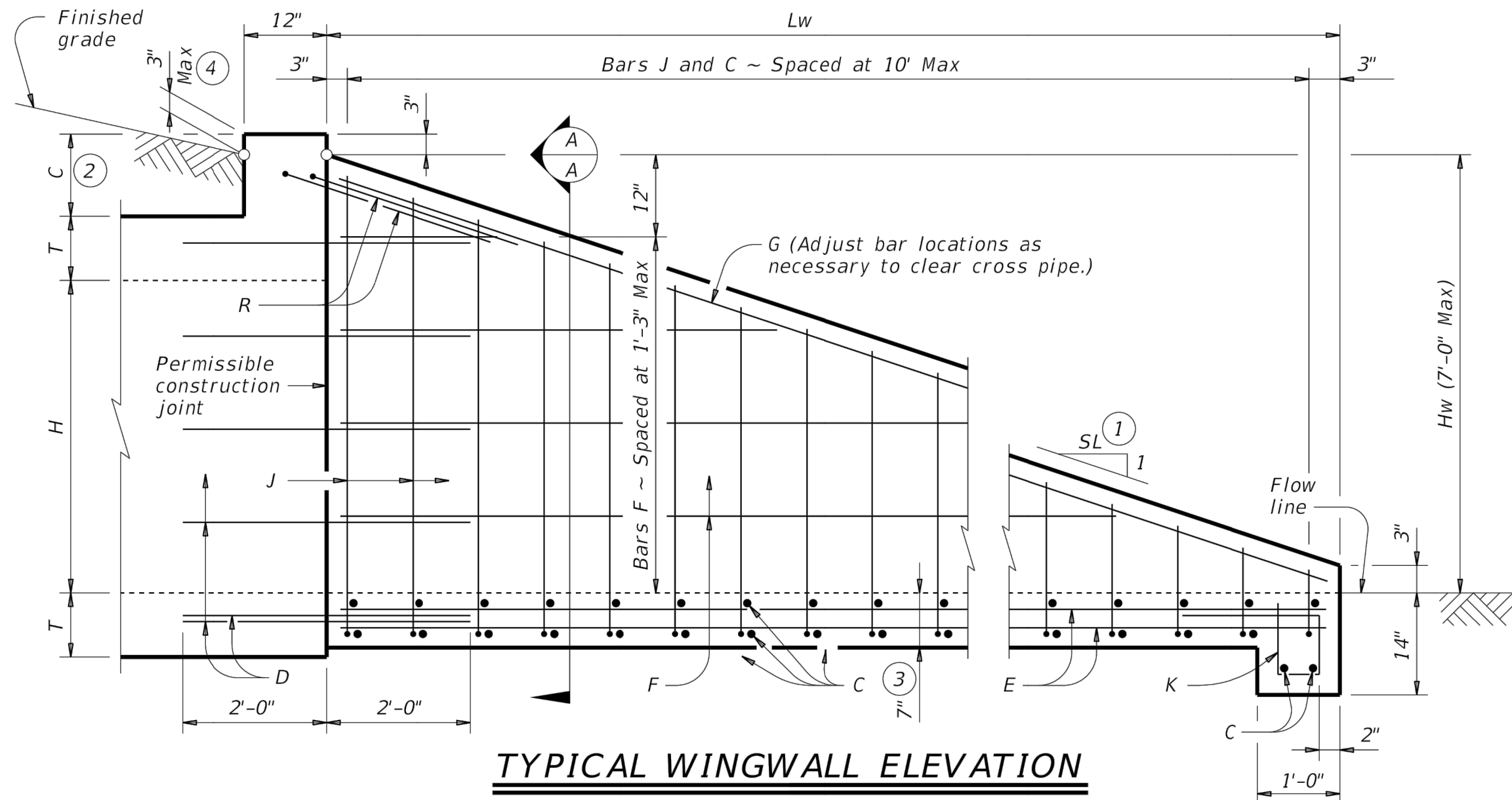
SETB-PD

FILE:	setbpdse-22.dgn	DN:	GAF	CK:	CAT	DW:	TxDOT	CK:	TxDOT
©TxDOT	February 2020	CONT	SECT	JOB	HIGHWAY				
REVISIONS									
06-2022	Wing dimensions	DIST	COUNTY	SHEET NO.					



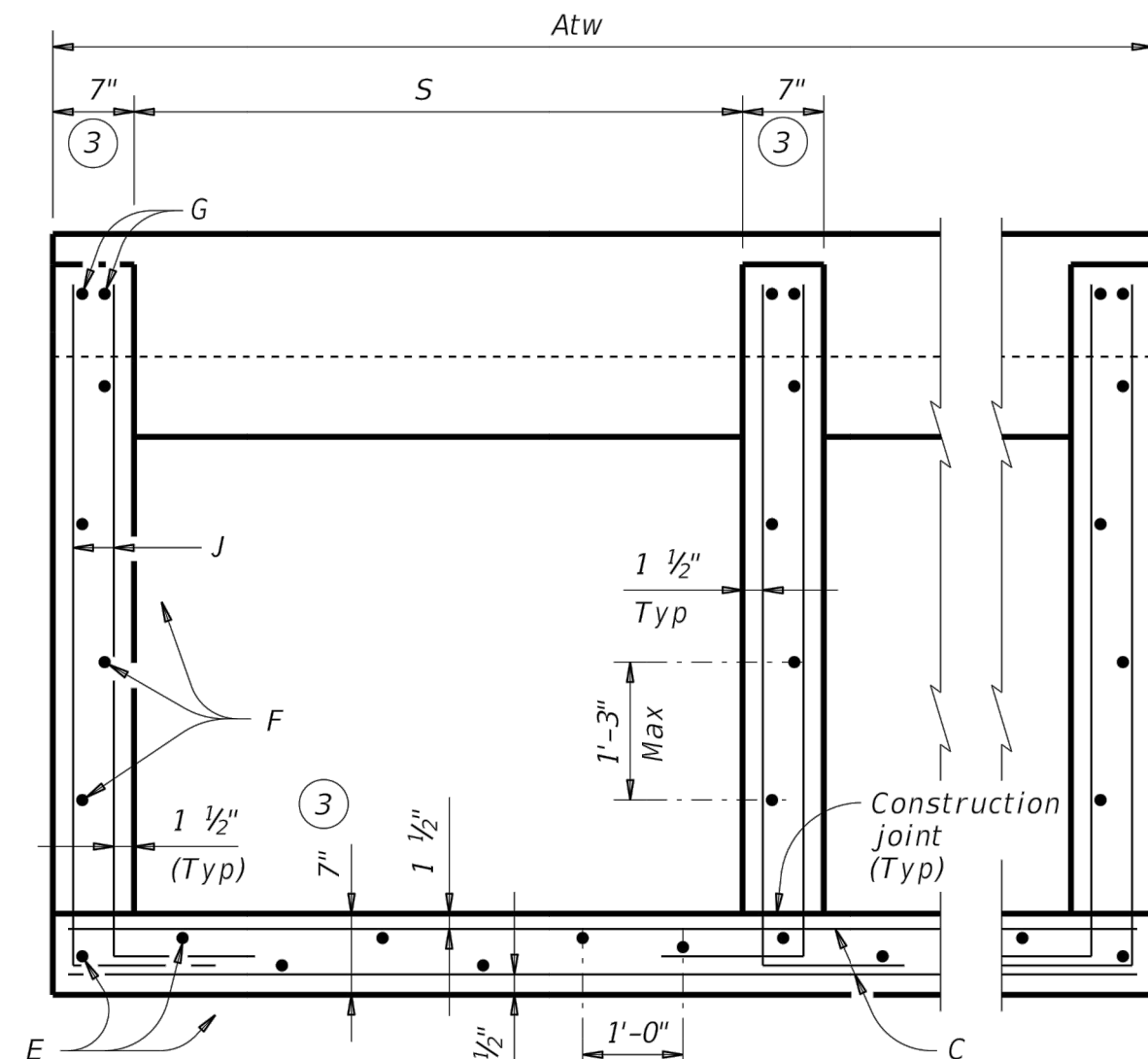
**ISOMETRIC VIEW OF TYPICAL INSTALLATION**

(Showing bolted anchor option.)



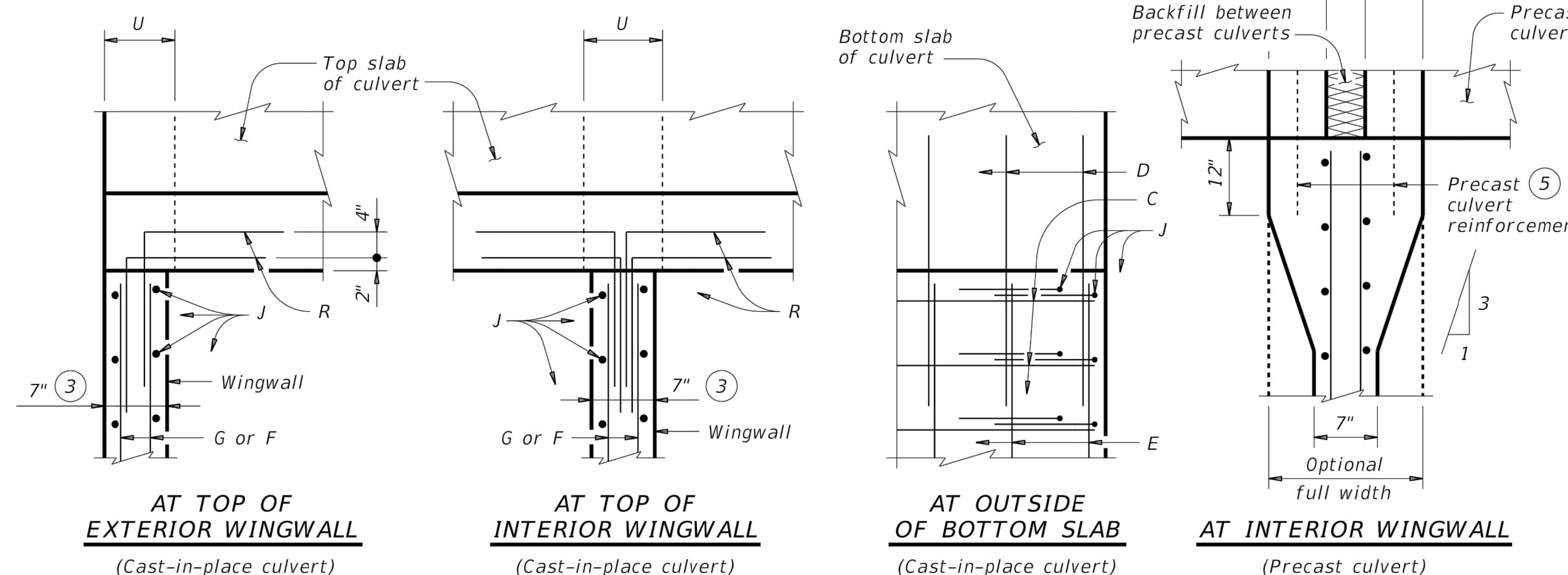
**TYPICAL WINGWALL ELEVATION**

(Cross pipes not shown for clarity.)



**SECTION A-A**

(Showing typical wingwall and wing slab reinforcing. Pipe runners not shown for clarity.)

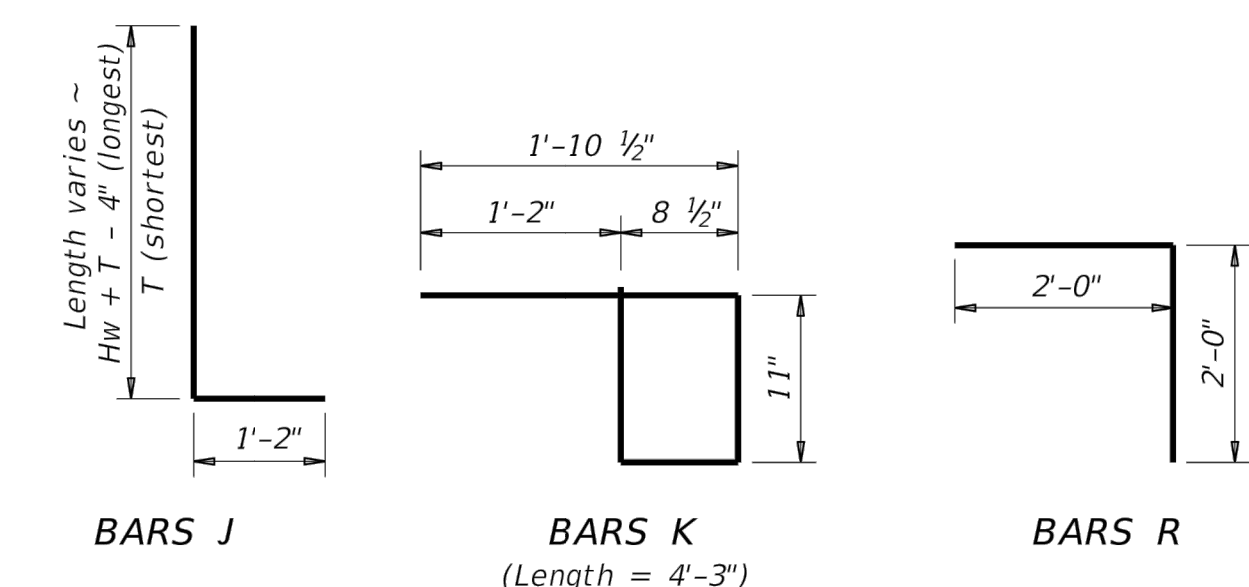


**PLAN VIEWS OF CORNER DETAILS**

**TABLE OF REINFORCING BAR SIZES AND SPACING**

Bar	Size	Spacing
C	#4	10" Max
D	#4	Match F and E
E	#4	1'-0" Max
F	#4	1'-3" Max
G	#6	As shown
J	#4	10" Max
K	#4	1'-0" Max
R	#4	As shown

- Provide 6:1 or flatter slope.
- 0' Min to 5'-0" Max. Estimated curb heights are shown elsewhere in the plans. For structures without railing and curbs taller than 1'-0", refer to Extended Curb Details the Extended Curb Details (ECD) standard sheet.
- Wingwall and slab thicknesses may be the same as the adjacent culvert wall and slab thicknesses (7" Minimum). If thicknesses greater than the minimum (7") are used, no changes will be made in quantities and no additional compensation will be allowed.
- For vehicle safety, reduce height, if necessary, to provide a maximum 3" projection above finished grade. No changes will be made in quantities and no additional compensation will be allowed for this work.
- For culverts with C = 0", the precast culvert reinforcing may extend 1'-0" minimum into wingwall. Wingwall bars D and R may be omitted. Otherwise, refer to the Wingwall Connection detail on the Box Culvert Precast Miscellaneous Details (SCP-MD) standard sheet.



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DATE:  
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REV.	DATE	DESCRIPTION	BY
1	8/10/23	ADDENDUM NO. 1	AMJ

ADDISON AIRPORT (ADS)  
ADDISON, TX  
ADDISON AIRPORT  
TAXIWAY B EXTENSION  
PHASE I

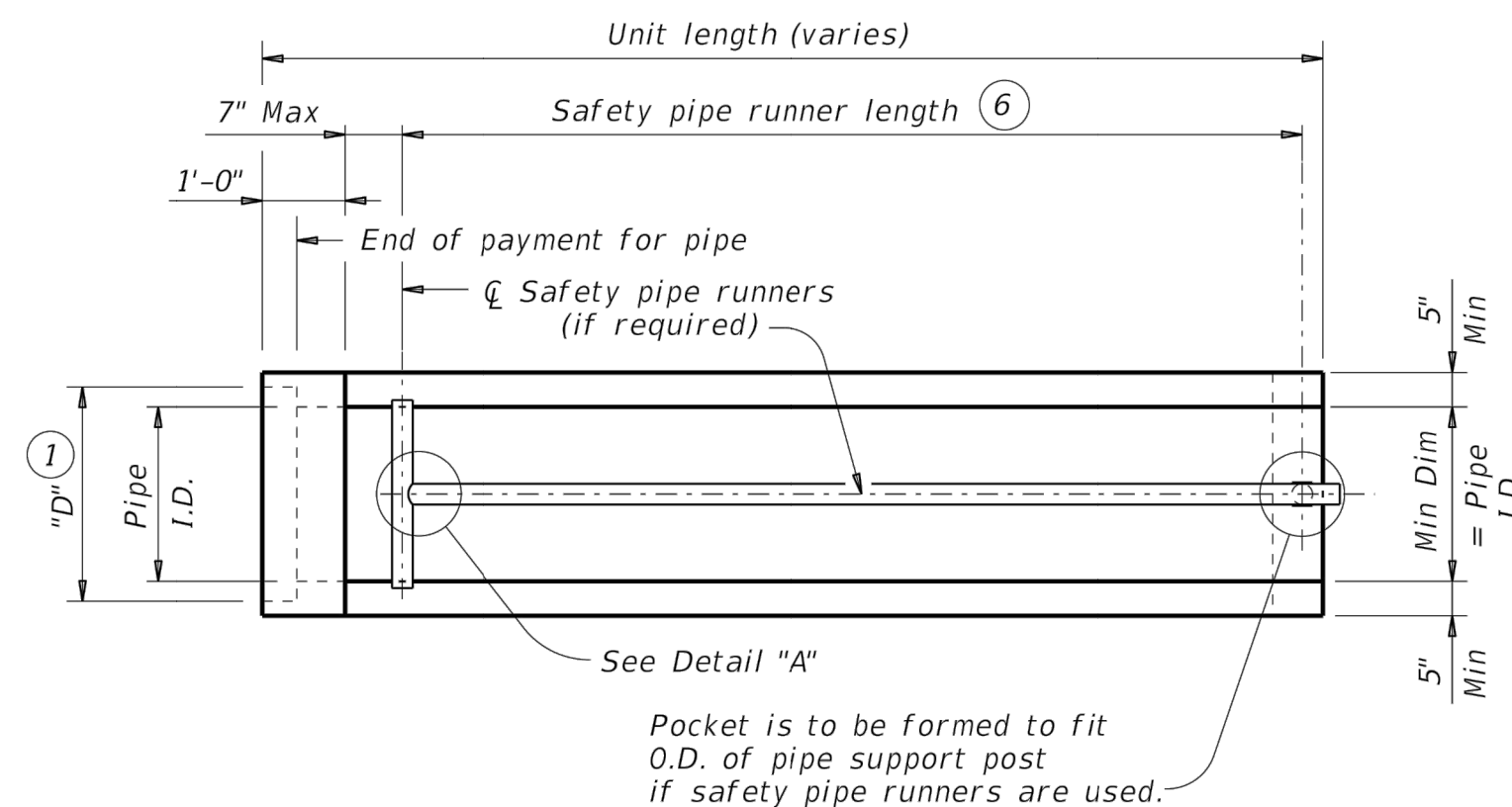
STORM DRAIN  
DETAILS 4

JOB NO.: 22A1112  
DATE: JULY 2023  
DESIGNED BY: RJT  
DRAWN BY: RJT

BAR IS ONE INCH ON ORIGINAL DRAWING  
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DRAWING NUMBER  
**CC-504**  
SHEET NUMBER  
**40**

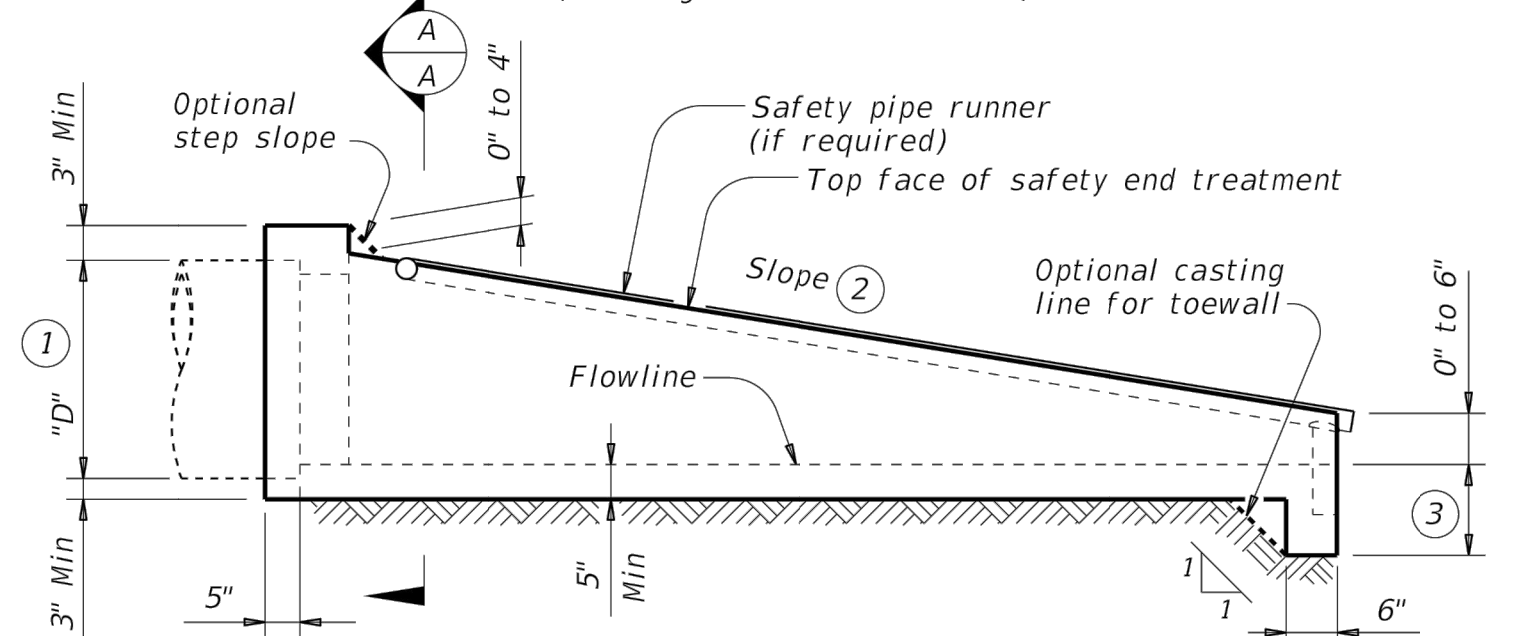
### REQUIREMENTS FOR CULVERT PIPES AND SAFETY PIPE RUNNERS

Pipe I.D.	RCP Wall "B" Thickness	TP Wall Thickness (8)	"D" (1)	Slope	Min Length of Unit	Single Pipe		Multiple Pipes			
						Skew	Pipe Runners Required	Skew	Pipe Runners Required		
12"	2"	1.15"	17.00"	3:1	2' - 11"	≤ 45°	No	≤ 45°	No		
				4:1	3' - 6"						
				6:1	4' - 9"						
15"	2 1/4"	1.30"	20.50"	3:1	3' - 8"	≤ 45°	No	≤ 45°	No		
				4:1	4' - 7"						
				6:1	6' - 5"						
18"	2 1/2"	1.60"	24.00"	3:1	4' - 6"	≤ 45°	No	≤ 45°	No		
				4:1	5' - 8"						
				6:1	8' - 0"						
24"	3"	1.95"	31.00"	3:1	6' - 2"	≤ 45°	No	= 30°	No		
				4:1	7' - 10"			> 30°	Yes		
				6:1	11' - 3"						
30"	3 1/2"	2.65"	38.50"	3:1	7' - 10"	= 15°	No	= 15°	No		
				4:1	10' - 1"					> 15°	Yes
				6:1	14' - 8"						
36"	4"	2.75"	45.50"	3:1	9' - 5"	= 0°	No	= 0°	Yes		
				4:1	12' - 3"					> 0°	Yes
				6:1	17' - 11"						
42"	4 1/2"	2.7"	52.50"	3:1	11' - 1"	= 0°	Yes	= 0°	Yes		
				4:1	14' - 5"					> 0°	Yes
				6:1	21' - 2"						



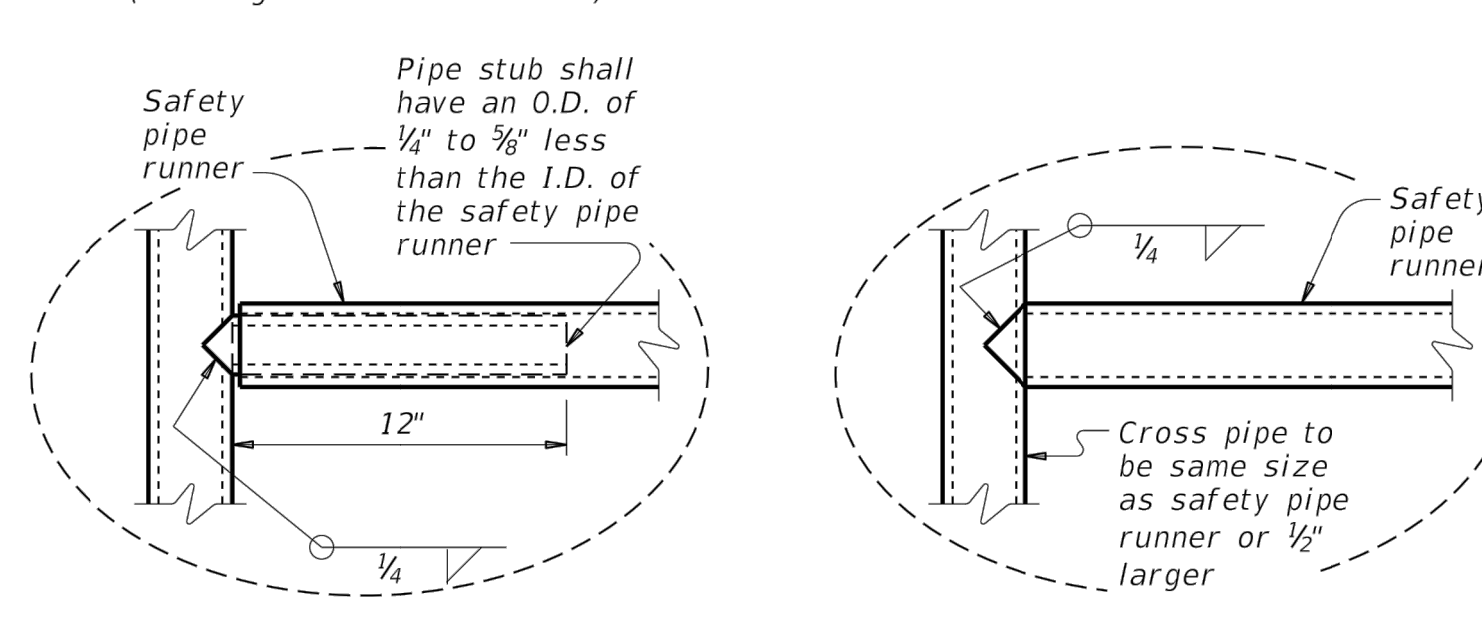
#### PLAN

(Showing bell end connection.)



#### LONGITUDINAL ELEVATION

(Showing bell end connection.)

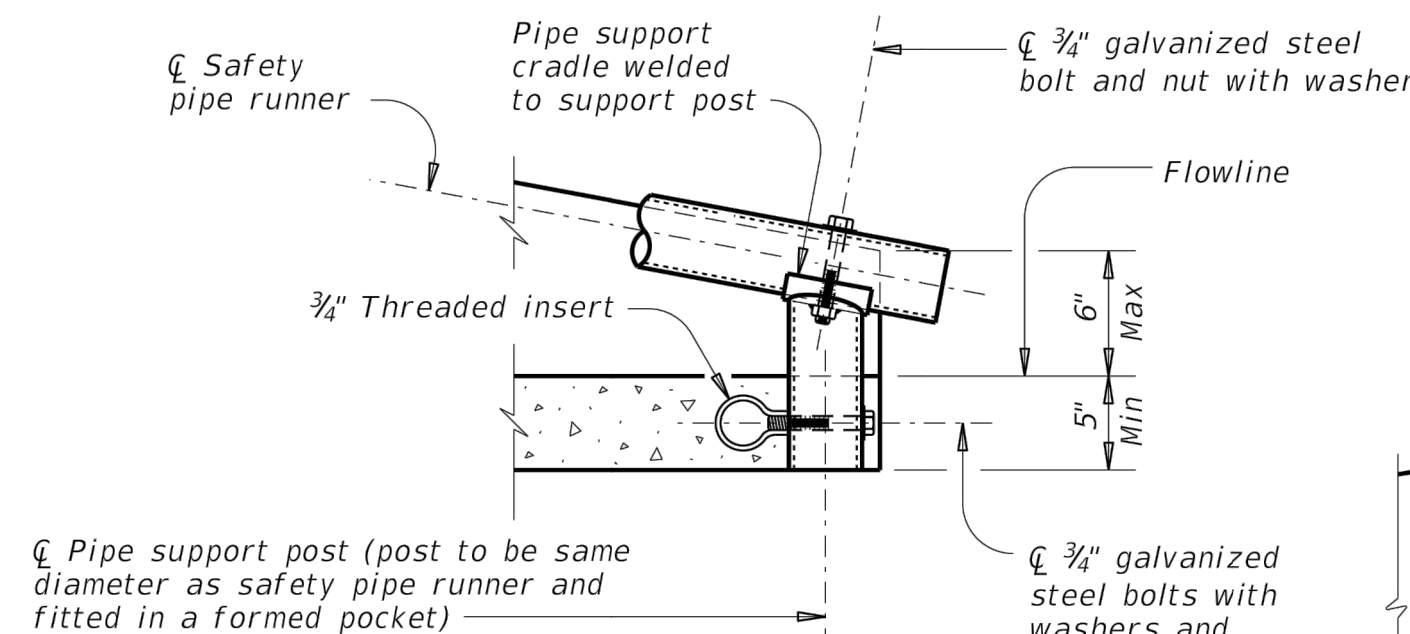


#### OPTION A

#### DETAIL A

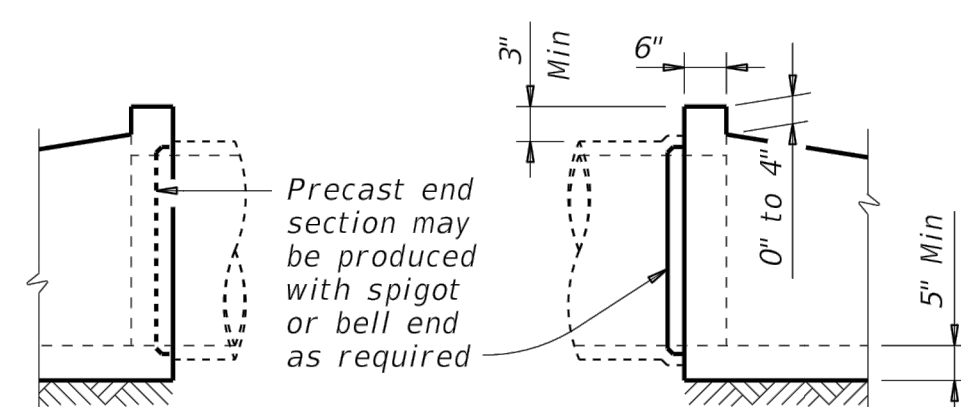
#### OPTION B

(If required)



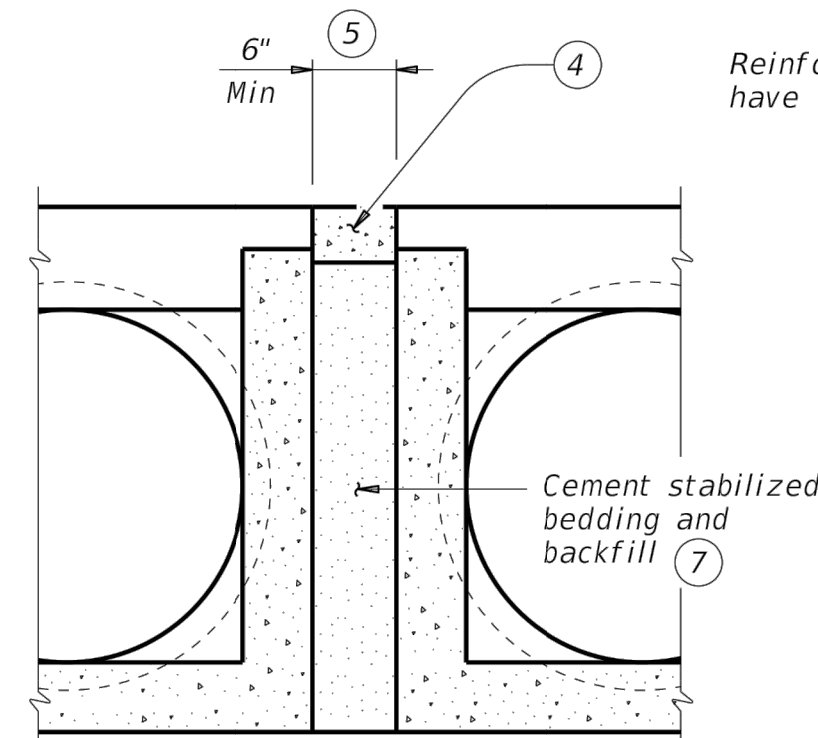
#### END DETAIL FOR INSTALLATION OF SAFETY PIPE RUNNERS

(If required)

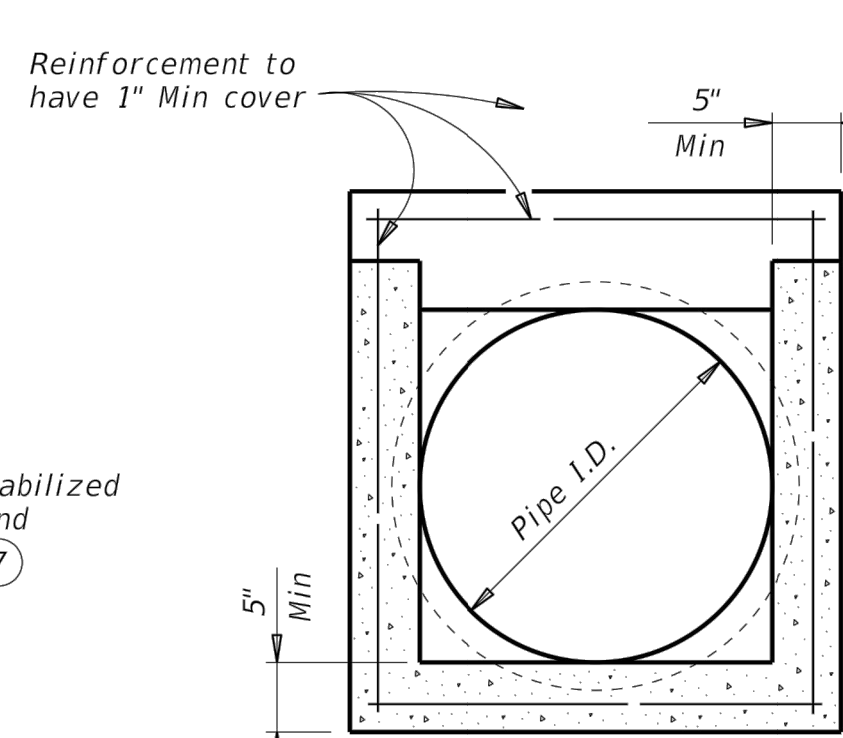


#### OPTIONAL JOINT FOR RCP

(Showing joint between RCP and precast safety end treatment)

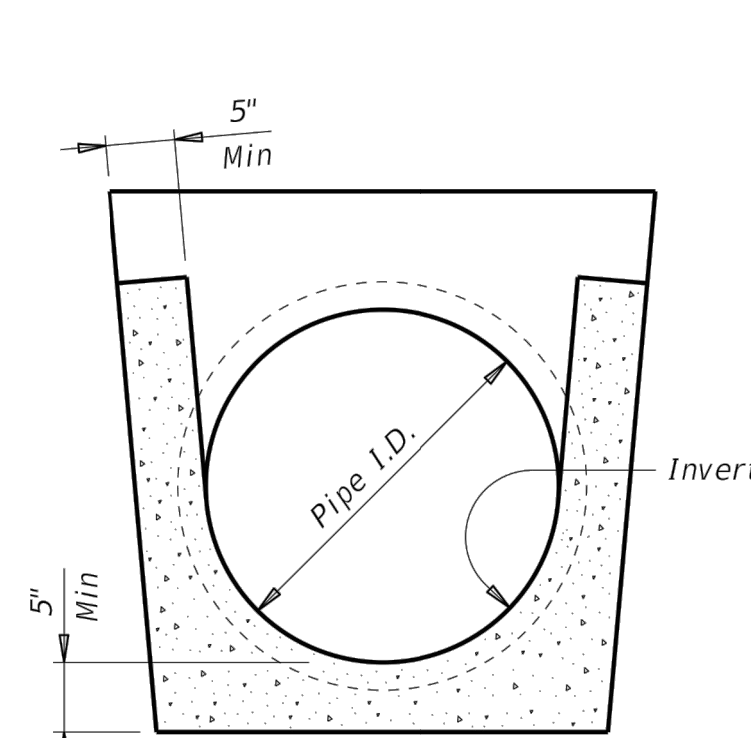


#### MULTIPLE PIPE INSTALLATION

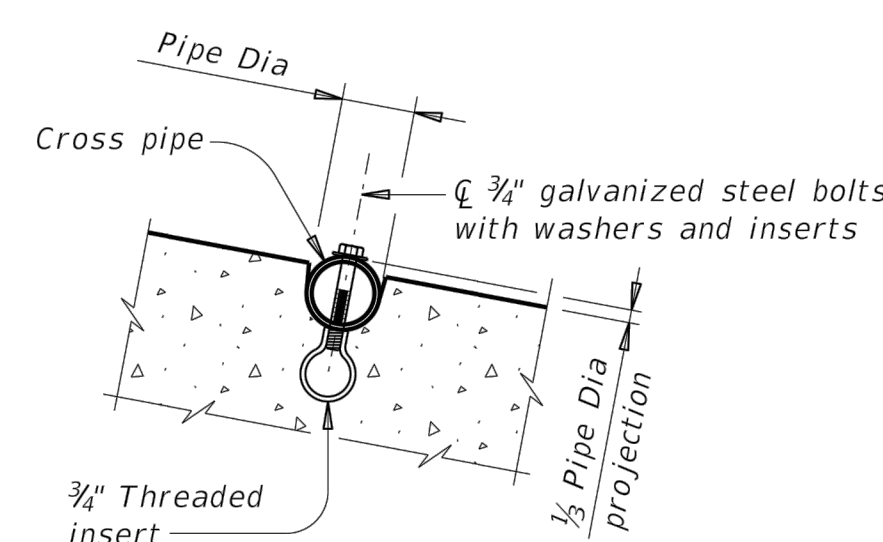


#### OPTION WITH SQUARE BOTTOM

#### SECTION A-A



#### OPTION WITH INVERT BOTTOM



#### INSTALLATION DETAIL FOR SAFETY PIPE RUNNERS

(If required)

- Dimension "D" is based on reinforced concrete pipe (RCP) meeting the requirements of ASTM C-76, Class III, (RCP Wall "B" thickness). Adjust "D" for any other wall thickness used. For thermoplastic pipe (TP) take into account the annular space requirements for grouted connections.
- Slope as shown elsewhere in plans. Slope of 3:1 or flatter is required for vehicle safety.
- Toewall to be used only when dimension is shown elsewhere in the plans.
- Fill the top 4" of void between precast end treatments with concrete riprap. Concrete riprap is considered subsidiary to the Item 467, "Safety End Treatment".
- Adjust clear distance between pipes to provide for the minimum distance between safety end treatments.
- Measured along slope.
- Provide cement stabilized bedding and backfill in accordance with the Item 400, "Excavation and Backfill for Structures". Bedding and backfill is considered subsidiary to the Item 467, "Safety End Treatment". When concrete riprap is specified around the safety end treatment, backfill as directed by Engineer.
- Thermoplastic pipe wall thickness may vary. Adjust accordingly. Thermoplastic pipe requires the safety end treatments to have a bell end for grouted connections.

#### GENERAL NOTES:

Precast safety end treatment for reinforced concrete pipe (RCP), and thermoplastic pipe (TP) may be used for TYPE II end treatment as specified in Item "Safety End Treatment".  
When precast safety end treatment is used as a Contractor's alternate to mitered RCP, riprap will not be required unless noted otherwise on the plans.  
Synthetic fibers listed on the "Fibers for Concrete" Material Producer List (MPL) may be used in lieu of steel reinforcing in riprap concrete unless noted otherwise.  
Manufacture this product in accordance with Item 467, "Safety End Treatment" except as noted below:  
A. Provide minimum reinforcing of #4 at 6" (Grade 40) or #4 at 9" (Grade 60) each way or 6"x6" - D12 x D12 or 5"x5" - D10 x D10 welded wire reinforcement (WWR).  
B. For precast (steel formed) sections, provide Class "C" concrete (f'c = 3,600 psi).  
At the option and expense of the Contractor, the next larger size of safety end treatment may be furnished as long as the "D" dimension cast is that of the required size of pipe.  
Pipe runners are designed for a traversing load of 1,800 Lbs at yield as recommended by Research Report 280-1, "Safety Treatment of Roadside Cross-Drainage Structures", Texas Transportation Institute, March 1981.  
Provide safety pipe runners, cross pipes, pipe support posts, and pipe stubs meeting the requirements of ASTM A53 (Type E or S, Grade B), ASTM A500 (Grade B), or API 5LX52.  
Galvanize all steel components except reinforcing steel after fabrication. Repair galvanizing damaged during transport or construction in accordance with the specifications.  
Connect RCP using the Optional Joint for RCP detail shown or in accordance with Item 464 "Reinforced Concrete Pipe". Connect TP by grouting. See Pipe and Box Grouted Connections (PBG) standard for grouted connections with TP and precast safety end treatment.

Texas Department of Transportation  
Bridge Division Standard

### PRECAST SAFETY END TREATMENT

#### TYPE II ~ CROSS DRAINAGE

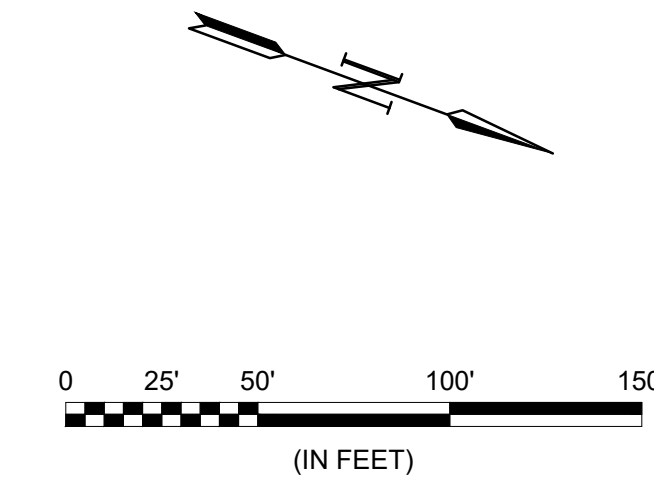
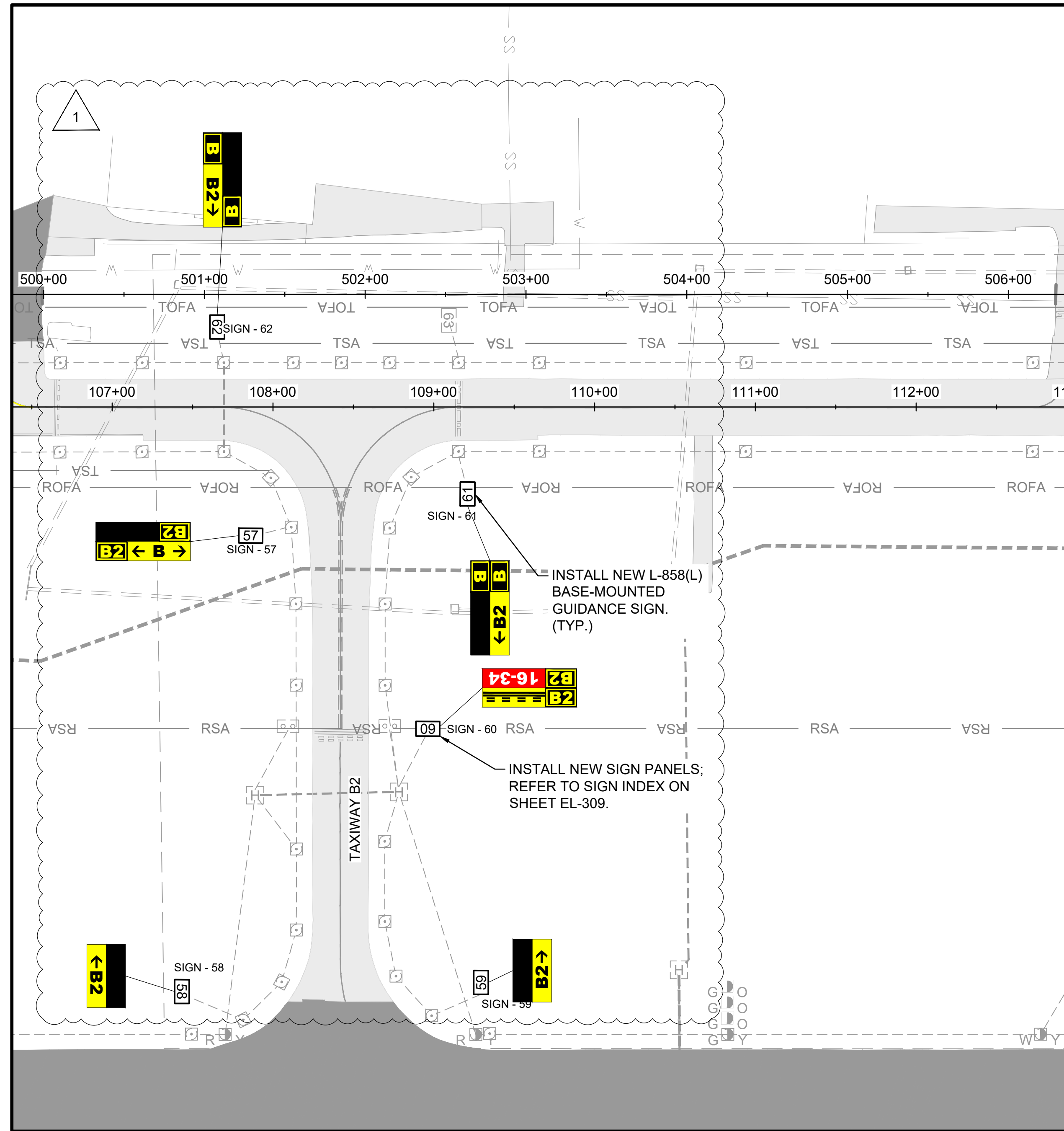
#### PSET-SC

FILE: psetscs-21.dgn	DN: RLW	CK: KLR	DW: JTR	CK: GAF
© TXDOT February 2020	CONT	SECT	JOB	HIGHWAY
REVISIONS	DIST	COUNTY	SHEET NO.	
12-21: Added 42" TP				

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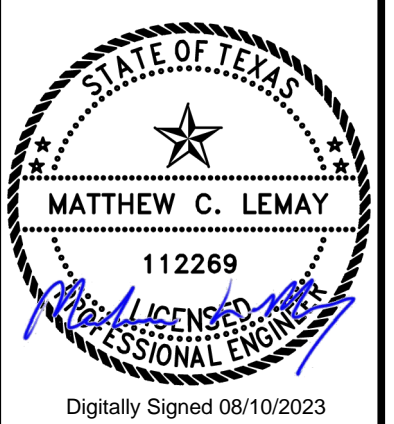


- GENERAL NOTES:**
- SEE SHEET EN-001 FOR ELECTRICAL INSTALLATION LEGEND, CONSTRUCTION NOTES, CAUTION NOTES, ELECTRICAL SAFETY NOTES, AND INSTALLATION NOTES.
  - SEE THE EL-300 SERIES FOR LIGHTING INSTALLATION DETAILS.
  - EXISTING CIRCUIT ROUTING IS APPROXIMATE. IF ROUTING DIFFERS IN THE FIELD, COORDINATE WITH ENGINEER.



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REGISTRATION NO. F-5713



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REV.	DATE	DESCRIPTION	BY
1	8/10/23	ADDENDUM NO. 1	MCL

ADDISON AIRPORT (ADS)  
 ADDISON, TX  
  
 TAXIWAY B EXTENSION  
 PHASE I

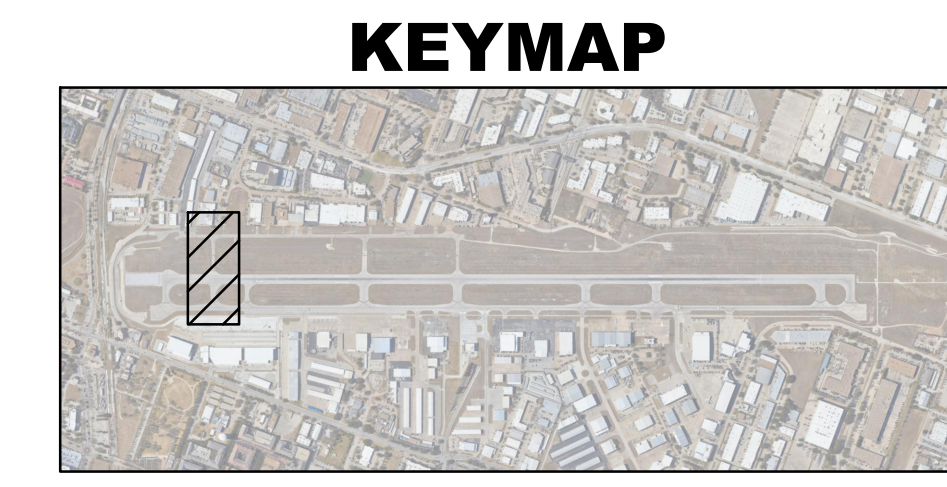
LIGHTING  
 INSTALLATION PLAN 1

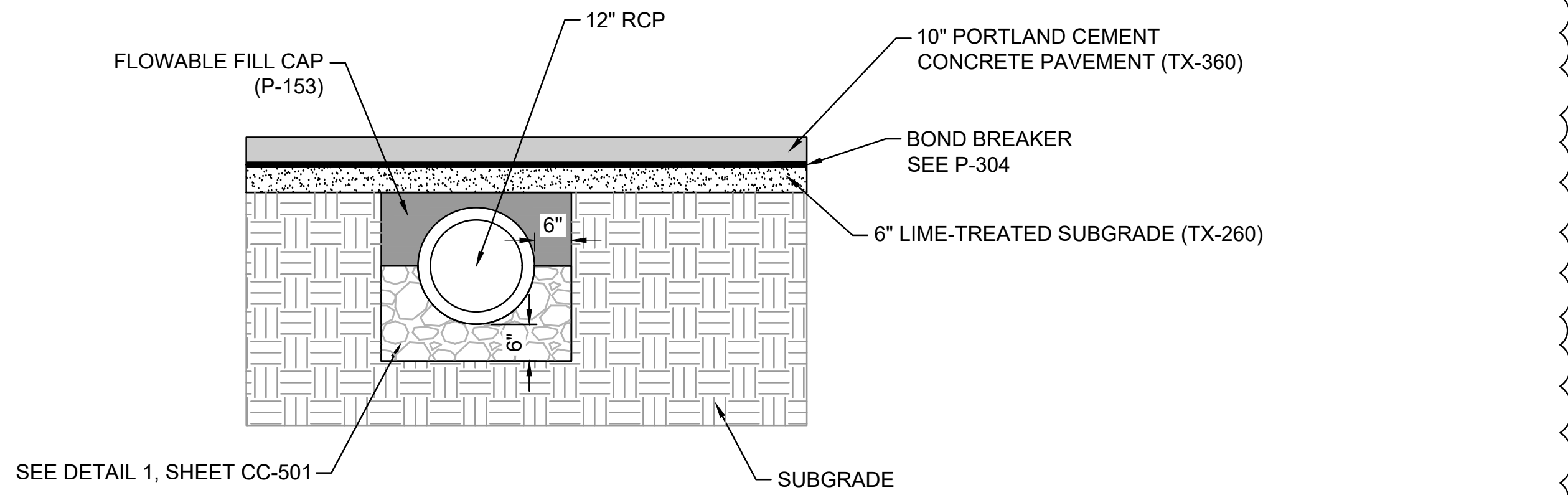
JOB NO.: 22A11112  
 DATE: JULY 2023  
 DESIGNED BY: RJT  
 DRAWN BY: RJT

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DRAWING NUMBER  
**EL-101**

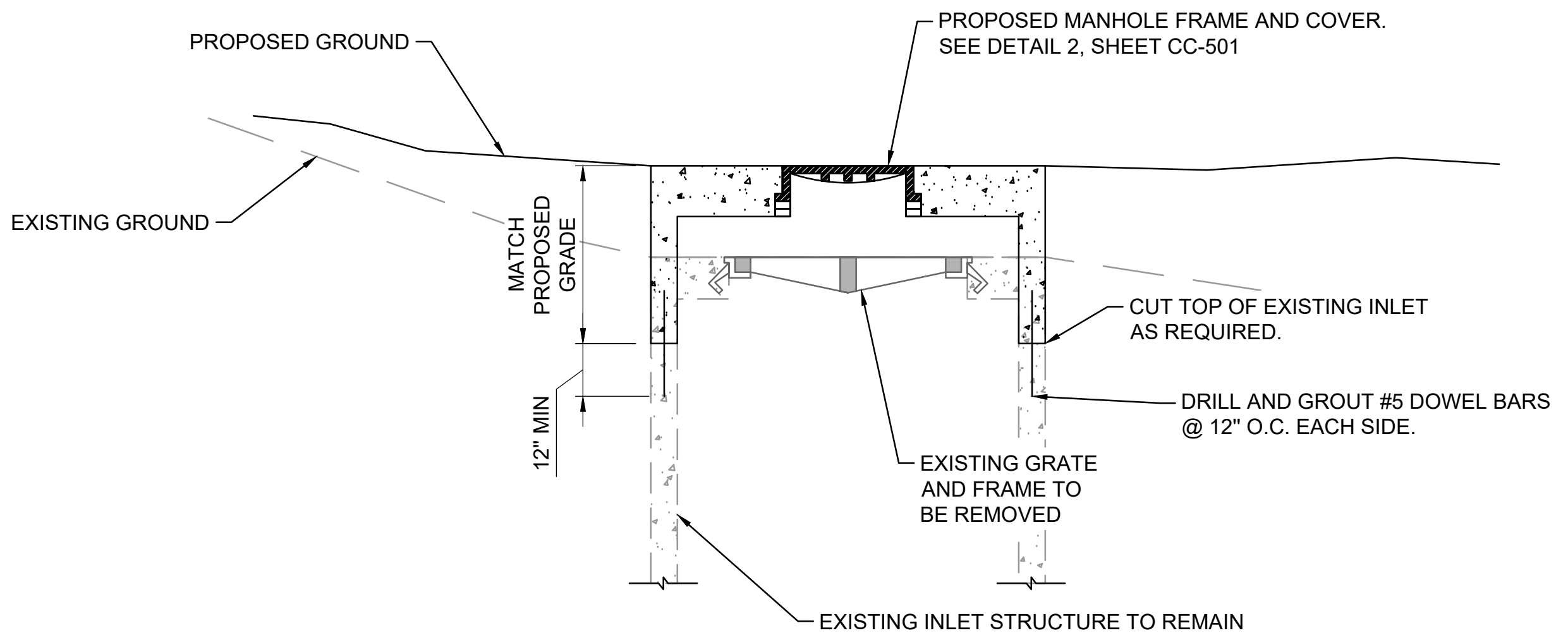
SHEET NUMBER  
**68**





**1 CONCRETE ENCASED PIPE BEDDING DETAIL**  
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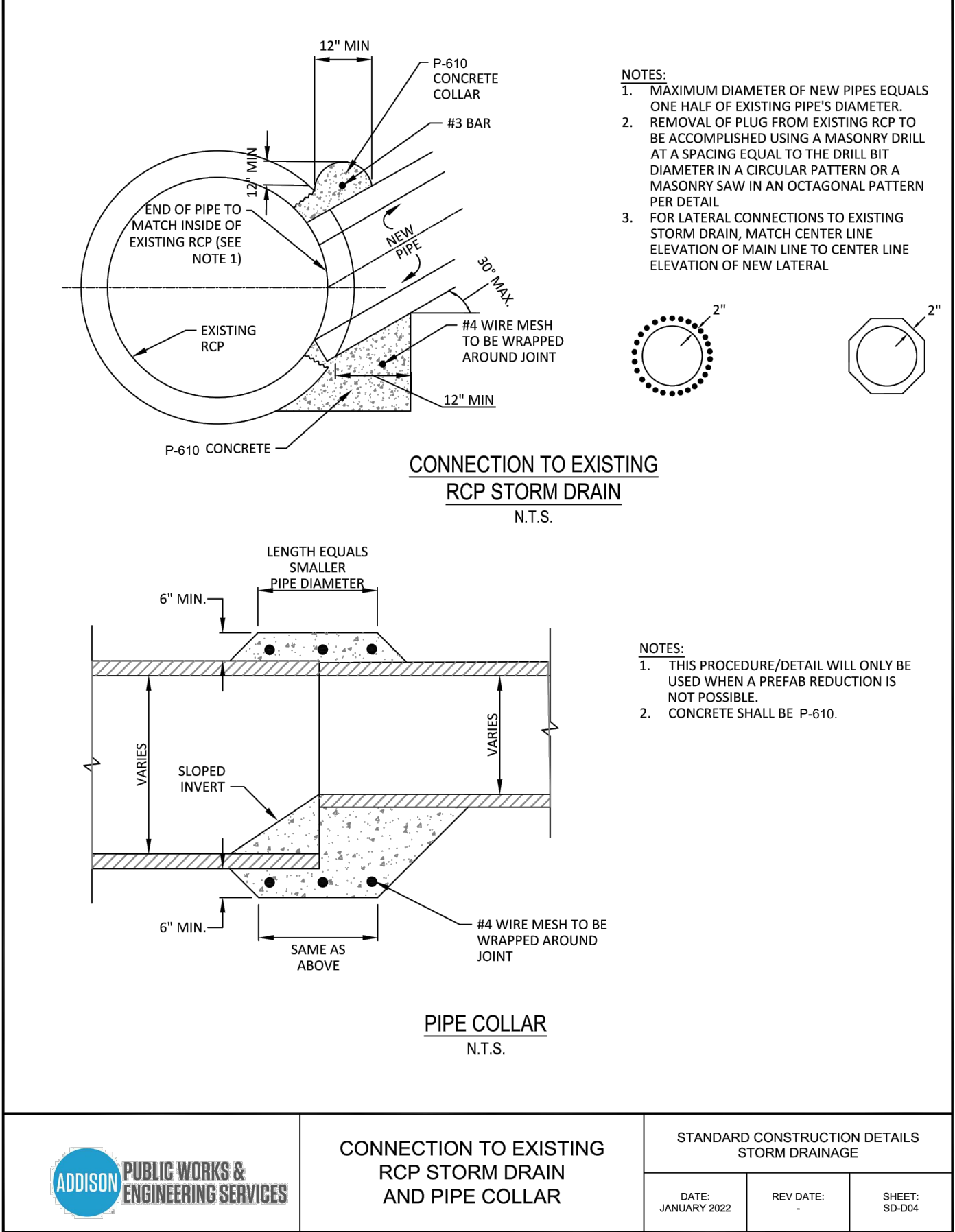
1. FLOWABLE FILL CAP SHALL ONLY BE PLACED WITHIN THE LIMITS OF THE PROPOSED PAVEMENT SECTION.



**2 INLET TO MANHOLE CONVERSION DETAIL**  
 SCALE: NONE

NOTE: SEE DETAIL 2, SHEET CC-505 FOR STEEL REINFORCEMENT.

MANHOLE DESIGN PARAMETERS	
MATERIAL	CAST IRON
OPEN AREA	24" MIN.
WHEEL LOADING	40,000 LBS MIN.

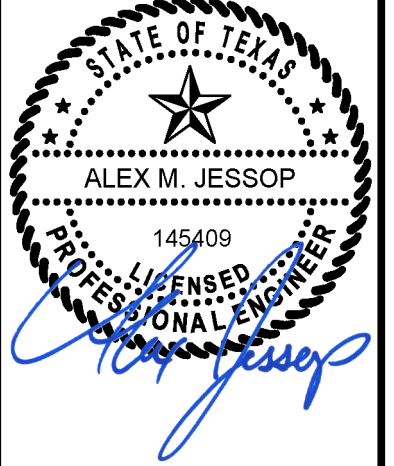


**3 CONNECTION TO EXISTING RCP**  
 SCALE: NONE



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REGISTRATION NO. F-5713



Digitally Signed 08/11/2023

REV.	DATE	DESCRIPTION	BY
1	8/10/23	ADDENDUM NO. 1	AMJ

ADDISON AIRPORT (ADS)  
 ADDISON, TX  
 TAXIWAY B EXTENSION  
 PHASE I

STORM DRAIN  
 DETAILS 1 - ADD ALT 2

JOB NO.: 22A11112  
 DATE: JULY 2023  
 DESIGNED BY: RJT  
 DRAWN BY: RJT

BAR IS ONE INCH ON ORIGINAL DRAWING  
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER  
**CC-501-2**

SHEET NUMBER  
**162**

File: L:\2022\22A11112 - ADS Taxiway Bravo Extension Phase I\Drawings\ADS\_TWB-CC-201.dwg Last Save: 8/10/2023 4:39 PM Last saved by: AMJ Jessop Last plotted by: Jessop, Alex M. Plot Style: AECmono.ctb Plot Scale: 1:1 Plot Date: 8/10/2023 4:52 PM Plotter Used: DWG To PDF.pc3